



MOTORSPORT AUSTRALIA MANUAL

2020





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WARNING

Motorsport activities are inherently dangerous recreational activities and there is significant risk of injury, disability or death.

If you choose to participate in these activities and you are killed or injured because the activities were not supplied with due care and skill or were not reasonably fit for their purpose, your rights to sue Confederation of Australian Motor Sport Ltd (CAMS) trading as Motorsport Australia and/or the Entities* are excluded, restricted or modified. For full details of the exclusion, restriction or modification of your rights please refer to motorsport.org.au/events/disclaimers.

*A full list of Motorsport Australia associated entities can be viewed at motorsport.org.au/events/disclaimers.

KEEP UP TO DATE ONLINE

All information in the 2020 Motorsport Australia Manual is correct at time of printing, however rules and requirements are likely to change throughout the year and those changes will not be reflected in this document.

To ensure you are equipped with the latest Motorsport Australia regulations, visit motorsport.org.au/regulations/manual.

The online Motorsport Australia Manual is the most up to date source of Motorsport Australia's rules and requirements, with amendments to the Manual clearly marked in the online PDFs.

Cover Photo:

2019 Motorsport Australia Rally Championship - Subaru Rally Tasmania - Luke Anear and Steve Glenney

Photo: Angryman Photography

Motorsport Australia Manual

Including National Competition Rules for 2020

OFFICIAL PUBLICATION OF THE CONFEDERATION OF AUSTRALIAN MOTOR SPORT LTD, TRADING AS MOTORSPORT AUSTRALIA

Founded as the Confederation of Australian Motor Sport on 1 February, 1953 and incorporated as CAMS Limited on 24 April, 1995, under Australian Company Number (ACN) 069 045 665. Changed name to Confederation of Australian Motor Sport Ltd (ACN 069 045 665) on 3 July, 2000. The trading name Motorsport Australia was officially implemented on 1 January, 2020.

Any references to CAMS in this publication refer to the Confederation of Australian Motor Sport Ltd (ABN 55 069 045 665), trading as Motorsport Australia.

Motorsport Australia Manual (61st Edition)

Previous editions have been published annually since 1959, except in 1963. ISSN 1033-0526

National Competition Rules (60th Edition)

Previous editions have been published in 1956, 1960 and 1962, then annually since 1964.

Authority

At a meeting of the Board of Motorsport Australia in December, 2019, it was resolved:

"That the National Competition Rules as submitted to the Board be and are approved as the National Competition Rules of Motorsport Australia as and from the first day of January, 2020. All previous Competition Rules in use in Australia, to the extent that such Rules conflict with the Rules presently submitted, being thereby superseded."

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Motorsport Australia has become aware that in recent years other organisations have adopted large parts of the Motorsport Australia Manual and National Competition Rules for events not sanctioned by Motorsport Australia and without contributing in any way to the ongoing cost and responsibility for their ongoing development and updating.

For this reason the following conditions of use will apply to the 2020 Edition of the Motorsport Australia Manual and National Competition Rules and all subsequent editions, releases and updates of them.

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Foreword



It is my pleasure to introduce the 2020 Motorsport Australia Manual.

2020 is a year of significant milestones and change for our organisation as we embark on an exciting new chapter for motorsport in Australia.

As you would now be aware, the Confederation of Australian Motor Sport (CAMS) has rebranded to Motorsport Australia in 2020 and beyond. This change has been in the planning for many months and we are excited about the opportunities this rebrand provides our organisation.

Of course, our history has not been forgotten. A key part of this rebrand was to make sure we acknowledge where we have come from and understand the significant role the CAMS name has played in motorsport in Australia. We will continue to acknowledge this history, and the people involved as we move forward under the Motorsport Australia name.

Ultimately, the Motorsport Australia rebrand will provide us with greater opportunities to grow our sport at all levels. This change brings us into line with the overwhelming majority of peak sporting organisations and will allow us to best represent our sport to governments and our commercial partners.

I am delighted to report that we had yet another year of growth in 2019, with a record number of licence holders and officials. This growth signifies that interest in our sport remains strong.

We are obviously buoyed by this continued growth, however we are aware that we face significant challenges in the year ahead to ensure the sport continues to thrive. Our new Strategic Plan, 'Empowering a Motorsport Nation' will help ensure the sport is well placed for the next three years and we have outlined a number of key strategic areas that will help shape the future of motorsport. You can read more about this plan on our website, motorsport.org.au.

Part of that plan is to ensure we make life as easy as possible for our members, and as of February this year we are pleased to be able to provide all members with access to a brand new online member portal and new event entry system. We hope these new systems will make your experience with Motorsport Australia more streamlined and user friendly. Again, I encourage you to visit our website to check out these new systems which will come online in the first few weeks of 2020.

On behalf of the Motorsport Australia Board and our hard-working administration, I wish you all the best for the year ahead and look forward to another successful and prosperous year of motorsport.

A handwritten signature in blue ink, appearing to read 'Eugene Arocca', with a stylized flourish at the end.

Eugene Arocca
Chief Executive Officer
EArocca@motorsport.org.au



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About Motorsport Australia

In 2020, the Confederation of Australian Motor Sport (CAMS) officially changed its trading name to Motorsport Australia.

The organisation has been the custodian of motorsport in Australia since 1953.

Motorsport Australia is the National Sporting Authority (ASN) for motorsport in Australia, and is delegated this responsibility by the Federation Internationale de l'Automobile (FIA).

Motorsport Australia affiliated with the FIA in its own right as CAMS in 1958 before being granted full membership in October of that year on a probationary basis.

In 1960 its membership of the FIA as an ASN was confirmed as permanent.

The FIA aims to ensure that motorsport is conducted in accordance with the highest standards of safety, fairness and social responsibility and Motorsport Australia, together with in excess of 120 other ASNs in over 100 nations, is committed to carrying out the mission of the FIA.

Motorsport Australia's Core Purpose

To be a regulator and FIA-delegated authority.

- Under the auspices of the FIA, continue to regulate the sport in a professional, streamlined and customer-focused manner as the delegated ASN. This will be achieved through the National Competition Rules and sporting and technical regulations commensurate with the level and type of event using the principle of simple rules for simple events and adequate rules for other events.
- Continue to promote the principles of safety, fairness and social responsibility in the conduct of motorsport within Australia.

To focus on sport and club development.

- Deliver leadership, innovation, customer focused and responsive services that positively contribute to

increased participation, interest, high performance and contribute to the social capital of Australian communities.

- Implement proactive sport, club, community and member development programs that provide an environment for our members that helps them experience motorsport in a positive, enjoyable and effortless manner.
- Focus on increasing equity and diversity within the sport and organisation through the delivery of proactive inclusion and integration programs that transcend race, religion, gender, age and creed, including but not limited to youth, women, disabled, indigenous and culturally diverse groups.
- Nurture an organisational culture and ethos that builds on club-level activity and promotes volunteerism and Motorsport Australia as an organisation of choice.

To foster and develop partnerships with other motoring bodies and stakeholders.

- Provide an operating environment that engages with and responds to the needs of our members, customers, stakeholders and communities. Foster and develop strategic partnerships locally, nationally and internationally that contribute to an integrated approach to achieving the objectives of Motorsport Australia and our partners.

To be a peak body for motoring enthusiasts.

- Proactively engage and encourage motor enthusiast bodies and individuals within Motorsport Australia using appropriate affiliation packages, programs and initiatives.



Motorsport Australia History

Motorsport Australia is a not-for-profit member-based organisation focused on the regulation, administration, development and promotion of motorsport across Australia.

The organisation has a proud history and has been the custodian of motorsport in Australia since 1953.

Motorsport throughout the world is directed and controlled by the FIA. Headquarters are at 8, Place de la Concorde, Paris, France. Motorsport Australia (previously CAMS) has been a member of the FIA since 1960 (then the CSI). The President of the FIA is Mr Jean Todt, and the Secretary-General is Mr Peter Bayer.

Founded in 1904, the FIA is a not-for-profit association. It brings together 219 national motoring and sporting organisations from 130 countries on five continents. Its member clubs represent over 100 million motorists and their families.

The FIA has been dedicated to representing the rights of motoring organisations and motor car users throughout the world via campaigns and activities that defend their interests. On issues such as safety, mobility, the environment and consumer law, the FIA actively promotes the interests of motorists at the United Nations, within the European Union and through other international bodies.

The FIA is also the governing body for motorsport worldwide. It administers the

rules and regulations for all international four-wheeled motorsport including the FIA Formula 1 World Championship, FIA World Rally Championship and FIA World Touring Car Championship. The FIA aims to ensure that motorsport is conducted in accordance with the highest standards of safety, fairness and social responsibility. So that the above standards may be exercised in a fair and equitable manner the FIA has drawn up the 'International Sporting Code'.

Motorsport Australia, together with more than 120 other ASNs in over 100 nations, is committed to carrying out the mission of the FIA. As such Motorsport Australia is responsible for the regulation and organisation of motorsport across Australia consistent with FIA regulations and processes.

In many countries the ASN is also the body recognised for all motoring activities, including sporting and touring. However in some countries, including Australia, the sporting power and touring authority are held by separate and independent bodies. The FIA's touring affiliate in Australia since 1972 is the Australian Automobile Association (AAA). Motorsport Australia in partnership with the AAA, delivers the FIA's objectives and programs across Australia.

Motorsport Australia is also the National Sporting Organisation for motorsport recognised by the Commonwealth Government through Sport Australia.

Getting Started

Step 1: See what's out there

Motorsport events are run almost every weekend of the year in each state, ranging from grassroots events like club-level hill climbs and motorkhanas, to international events like Rally Australia and the Formula 1 Grand Prix.

The Motorsport Australia website, motorsport.org.au, maintains a searchable calendar of upcoming events.

Step 2: Join a Motorsport Australia Affiliated Car Club

Joining a club helps you meet people who can assist you to get involved in motorsport. Many clubs run 'Come and Try' days and some even provide vehicles so that you can experience first-hand what motorsport competition is all about. Joining a club is also the first step along the path to obtaining a Motorsport Australia licence.

Step 3: Obtain a Licence

Motorsport Australia issues a number of different licences to cover every aspect of participation in motorsport, ranging from single day licences for people that wish to try out something new, up to full National and International Racing licences. Your club can advise you of the best licence for you.

Licence Information

Motorsport Australia offers a range of licences across all disciplines starting at entry level through to international.

Entry Level Licences

A Non-Speed licence entitles the competitor to compete in events such as observed section trials, touring assemblies, non-timed road events, motorkhanas, khanacross and drifting events, up to International level.

A Speed licence entitles the holder to compete in Non-Speed events plus regularity trials up to National Championship level, single and multi-car speed events (not racing) up to International level, and touring road events that do not run over closed road sections

Clubman Level

Motorsport Australia has a range of Clubman-level licences focused at those who wish to compete in a particular discipline up to and including state level event status.

National Level

National Licences are discipline specific – Race, Rally/Road and Off Road. Holding a Licence at National level in one discipline does not entitle you to compete in another discipline.

International Level

The FIA licence is required to compete in International events. Again discipline-specific, the licences have additional requirements to National licences. In accordance with FIA Regulations, these licences expire on 31 December every year.

Competitor Licences

Competitor licences are for competition vehicle owners who are not the holders of a driver/navigator licence.

Competitor licences may be issued to:

- individuals
- companies
- groups
- teams

The name on the Motorsport Australia Competitor Licence must correspond to the name in the log book of the relevant vehicle. Applications for competitor licences must, where relevant, submit evidence of incorporation and provide an ACN/ABN or registration of business name at the time of application.

Officials

Why Become a Motorsport Australia Official?

- Get close to the action
- Be part of the team
- Gain skills and experience
- Progress up the officiating pathway
- Choose a role that suits you
- Gain opportunities to travel
- Give back to the community
- Have fun!

How do I become a Motorsport Australia Accredited Official?

There are two ways to become a Motorsport Australia General Official:

Option 1: Trainee Official's Licence

Register online as an official at **motorsport.org.au/officials** - Motorsport Australia will then email you details of the on-line 'Introduction to Motorsport Australia Officiating' Module, which you are required to complete before your officials' licence is issued.

When successfully completed, you will be issued with a General Official's Licence which will be posted to you in the mail. Once you have this, get in touch with a car club or officials' association in order to start volunteering – Motorsport Australia Sport and Club Development Officers can put you in touch with a club in your area.

For your first event, you will need to work under supervision.

Option 2: Introductory Theory Module

If you sign up on the day of an event, you will need to complete the On The Day Officials' Sign Up Form, and you will be given a tear off slip. This slip acts as your licence for a period of thirty (30) days. At this event you will need to work under supervision.

Following the event, your licence form will be sent to Motorsport Australia, at which point you will be sent the on-line 'Introduction to Motorsport Australia Officiating' Module.

Upon successful completion of the module you will be mailed out your Motorsport Australia General Official's Licence.

PLEASE NOTE: Victorian Officials are also required to have a valid Working with Children Check

Motorsport Australia National Officiating Program

The National Officiating Program (NOP) was established for the grading, licensing, training and recognition of volunteer motorsport officials.

The purpose of the NOP is to:

- ensure that motorsport officials are highly skilled
- provide knowledge of roles and rules
- demonstrate ethical performance of duties
- foster personal pride and a respect for the sport
- provide efficient management
- achieve local, national and international recognition.

Training under the NOP

Motorsport Australia provides a number of training courses and programs to accredit volunteer officials under the NOP. This ensures officials have the correct knowledge and skill set required for the role they perform. All Motorsport Australia Accredited Official Training Modules are endorsed by the Sport Australia under the National Officiating Accreditation Scheme (NOAS). You can read more about the NOAS by visiting the Sport Australia website.

The NOP also provides the framework for the structure and grading of officials' licenses and the process of obtaining, maintaining and renewing a Motorsport Australia Official's Licence.

Club Affiliation Benefits

- Licencing and Event Permits
- Officials and Volunteer Training
- Motorsport Australia Event Entry
- Club Service and Membership Support
- Safety in Motorsport
- Venue Development Assistance
- Event promotion through Motorsport Australia's media channels
- Access to Motorsport Australia's National Insurance Coverage. All motorsport and some social events staged by a Motorsport Australia affiliated club will require a Motorsport Australia permit to be issued for this coverage to be in force

Motorsport Australia Sport and Club Development Team

Access to Motorsport Australia's Sport and Club Development Team, which is available to assist clubs with issues facing grassroots sporting organisations.

Collaborate with Clubs

Opportunity to collaborate with other Motorsport Australia affiliated car clubs to run events in order to build and maintain a sustainable club network.

International Recognition

Motorsport Australia is delegated its responsibility by the international

governing body for motorsport, the FIA, and is also recognised by the Australian Government as the National Sporting Authority for motorsport in Australia.

Club Voice

Representation and ability to raise and debate issues at the State Council in your club's jurisdiction, ensuring your club is kept up to date on matters both within your club's jurisdiction and nationally.

Integrated International System of Motorsport Regulation

Operate under an integrated international system of motorsport regulation that provides stable, universal standards of competency, organisational capacity, fairness and judicial process for all participants.

Both sporting and enthusiast car clubs are important to Motorsport Australia and, as such Motorsport Australia provides affiliation options for all types of car clubs.

Go to ***motorsport.org.au/clubs/affiliation*** for more information about Motorsport Australia Sporting and Enthusiast Club affiliation products.

If you would like more information about affiliating with Motorsport Australia, contact us on 1300 883 959 or development@motorsport.org.au.



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International Control of Motorsport

Motorsport

Motorsport throughout the world is directed and controlled by the Federation Internationale de l'Automobile (FIA).

Its headquarters are at 8, Place de la Concorde, Paris, France.

Motorsport Australia has been a member of the FIA since 1960 (then the CSI), and since 1972 Australia has also been represented on the touring side of the FIA by the Australian Automobile Association (see following). The President of the FIA is Mr Jean Todt, and the Secretary-General is Mr Peter Bayer.



Jean Todt
President of FIA

Motorsport Australia's responsibilities to the FIA

Motorsport Australia is the sole authority appointed by the FIA to regulate four-wheeled motorsport in Australia in order to promote and achieve:

- safety
- fairness
- social responsibility in the conduct of the sport

This includes the application of the FIA's International Sporting Code and the conduct of motorsport activities in Australia which will bring credit to the sport internationally.

Touring aspects of motoring

General motoring enquiries other than of a sporting nature should be directed to the FIA's touring affiliate in Australia, the Australian Automobile Association:

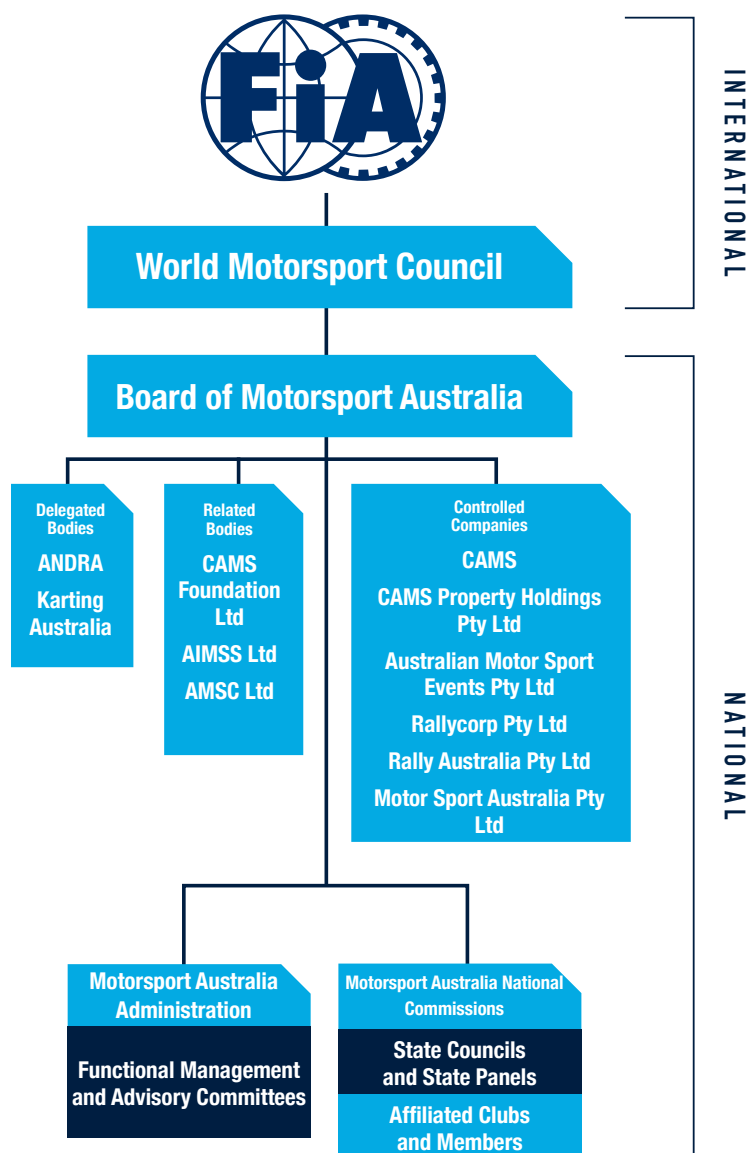


AUSTRALIAN
AUTOMOBILE
ASSOCIATION

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Chief Executive
Australian Automobile Association
PO Box 1555
Canberra ACT 2601

Ph: (02) 6247 7311
Web: aaa.asn.au

Motorsport Australia Structure



The Board of Motorsport Australia

President



Andrew Papadopoulos

2009 - 2021

APapadopoulos@motorsport.org.au

Directors



Terry Atkinson

Elected Director: Tasmania
2014 - 2021



Graeme Emerton

Vice President,
Elected Director: New South Wales
2014 - 2021



Margot Foster AM

Vice President,
Appointed Director
2018 - 2020



Andrew Fraser

Appointed Director
2019 - 2021



Norman Gowers

Elected Director: Victoria
2013 - 2021



Thea Jeanes-Cochrane

Appointed Director
2019 - 2022



Kristen Bailey

Elected Director: South Australia/
Northern Territory
2020



Nick Rahimtulla

Elected Director: Western Australia
2017 - 2022



Laurence Svenson

Elected Director: Queensland
2009 - 2020

All correspondence to Motorsport Australia Directors can be sent to:

Motorsport Australia National Office, 275 Canterbury Rd,
Canterbury VIC 3126. Ph: 1300 883 959.

State Council Chairs



**New South Wales /
Australian Capital Territory**

Jon Thomson



Queensland

Paul Woodward



**South Australia /
Northern Territory**

Adrian Flynn



Tasmania

Kim Taylor



Victoria

Jess Harper



Western Australia

Tracey Rowley

All correspondence to Motorsport Australia State Council Chairs should be directed to the relevant Motorsport Australia Sport and Club Development location, as listed on page 19.

Past Presidents



1953 Founding President
M H Monk

From 1954 to 1962 the Presidency rotated, being determined by the State in which that year's Australian Grand Prix was held.

1954 S C Mossetter

1955 W D Vervo

1956 M H Monk

1957 M Maurice

1958 A M Hayes

1959 O Grahame

1960 W L Pitt

1961 G A Biddle

1962 R Style

From 1963 to 2001, the President was independently elected by the Board each year.

1963 - 1968 O Grahame

1969 - 1971 L D Stewart

1972 - 1975 B J Dunstan

1976 R L Dearie

1977 - 1982 J B Roxburgh

1983 - 1994 J M Large

1994 - 1995 D H Tait

1996 - 1998 P J S Bready

1999 - 2001 J N Osborn

2001 C Osborne

From 2002, the President has been elected for a two-year term.

2002 - 2008 C Osborne

From 2018, the President has been elected for a three-year term.

2009 - A Papadopoulos

President of Honour

In 1994, Motorsport Australia created the position of 'President of Honour' and conferred it upon its longest-serving president, after his retirement from that position.

1994 - 2006 John M Large

Chairman of Honour

In 1968, the position of 'Chairman of Honour' was created and conferred upon the first independent President after his retirement from the position.

1963 - 1968 Owen Grahame

Contacting Motorsport Australia

National Office

275 Canterbury Road, Canterbury, Victoria 3126
PO Box 172, Canterbury LPO, Victoria 3126

Phone: 1300 883 959 | **Fax:** (03) 9593 7700

Email: info@motorsport.org.au | **Website:** motorsport.org.au

Motorsport Hotline

1300 883 959

for licencing and
general inquiries

State	Contact details	
New South Wales / Australian Capital Territory <i>(excluding the Counties of Townsend, Cadell, Yancowinna and Wakool, the Shires of Berrigan, Corowa and Greater Hume (NSW))</i>	P.O Box 6126, Silverwater BC, NSW, 1811 Ph: (02) 8736 1218	Contact us: nsw@motorsport.org.au Office hours: Monday to Friday 9.00am – 5.00pm
Queensland	PO Box 1859, Milton BC QLD 4064 Ph: (07) 3850 2400	Contact us: qld@motorsport.org.au Office hours: Monday to Friday 9.00am – 5.00pm
Western Australia	PO Box 2296, Yokine South WA 6060 Ph: (08) 9208 8500	Contact us: wa@motorsport.org.au Office hours: Monday to Friday 9.00am – 4.00pm
Victoria <i>(including the Counties of Townsend, Cadell and Wakool and the Shires of Berrigan, Corowa and Greater Hume)</i>	PO Box 172 Canterbury LPO Vic 3126 Ph: 1300 883 959	Contact us: vic@motorsport.org.au Office hours: Monday to Friday 9.00am – 5.00pm
South Australia / Northern Territory <i>(including County of Yancowinna, NSW)</i>	3/43 King William Street, Kent Town SA 5067 Ph: (08) 8361 4801	Contact us: sa@motorsport.org.au Office hours: Monday to Friday 9.00am – 5.00pm
Tasmania	PO Box 127, New Town TAS 7008 Ph: (03) 6227 5601	Contact us: tas@motorsport.org.au Office hours: Monday to Friday 8.00am – 4.00pm

Motorsport Australia Partners and Programs



Australian Institute for Motor Sport Safety

The Australian Institute for Motor Sport Safety (AIMSS), a not-for-profit organisation, was established in 2007 and is the peak body for motorsport safety and related research in Australia.

AIMSS works in conjunction with Motorsport Australia in an advisory role and is a research partner of the FIA Institute for Motor Sport Safety and Sustainability to develop safety through research, education and industry liaison.

As a result of its partnership status with the FIA Institute, AIMSS works to disseminate safety breakthroughs, news and information from around the world to the Australian motorsport community.

Contributions to AIMSS help keep Australia at the forefront of motorsport safety – benefiting Motorsport Australia members and licence holders at all levels.

aimss.com.au



Ricciardo's Racers

The Ricciardo's Racers program is an initiative recognised and supported by the FIA Sports Grant Programme and the Australian Government. The program is designed for motorsport participants aged 12-17 years of age, encouraging a younger generation to get involved in the many different forms of motorsport. Participants receive a Ricciardo's Racers member card and pack, along with an invitation to participate in a Ricciardo's Racers Junior Drive Day in their state.

motorsport.org.au/ricciardoracers

Club Development Fund

The Club Development Fund makes funding available to Motorsport Australia affiliated car clubs as part of our ongoing commitment to driving growth in club level motorsport. Funds are allocated to projects that fit within the funding areas, including come and try days, junior development programs and events and establishment of new events. The Club Development Fund is supported by Famous Insurance and Burson Auto Parts.

motorsport.org.au/clubs



Karting Australia

The Australian Karting Association Ltd trading as Karting Australia (KA) is the governing body for the sport of Karting in Australia. Formed in 1966 the AKA has produced some of Australia's finest motor racing talents.

The power and authority to organise and control the sport within Australia has been delegated to Karting Australia by the Federation Internationale de 'l Automobile (FIA) through Motorsport Australia.

Five-time Moto GP World Champion Mick Doohan is the Karting Australia Chairman with one of Australia's most experienced motorsport administrators Kelvin O'Reilly as Chief Executive Officer.

Under the guidance of the Board of Directors, Karting Australia's objectives are to encourage competition from a young age at appropriate levels, to develop excellence and to increase participation in the sport of Karting.

At all times the sport of Karting should have as its primary objective the achievement of the following functions and responsibilities:

- Increased participation in the sport through the development and implementation of proactive sport, club, community and member development programs;
- Implement a deliberate strategy to develop a consistent national regulatory

regime across all areas of activity which facilitates participation in any state without any additional requirements;

- Establish and foster partnerships with key stakeholders for the betterment of the sport of Karting;
- Focus on increasing equity and diversity within the sport and organisation, including but not limited to youth, women, disabled, indigenous and culturally diverse groups;
- Contribute to the development of a National Infrastructure and Facilities Strategy to ensure the sport has access to facilities and venues of an appropriate standard into the future;
- Proactively engage with and contribute to the aims and objectives of the KA as contained within the KA Corporate Plan.

The members of Karting Australia are the State Karting Associations of the six states of Australia and of the Northern Territory:

- Karting TAS Inc.
- Australian Karting Association (SA) Inc.
- Karting Australia (NSW) Inc.
- Australian Karting Association (QLD) Inc. t/as Karting QLD
- Karting (WA) Inc.
- Australian Karting Association (NT) Inc.

and such other incorporated Karting Associations which all existing members agree to admit to membership.

Contact details:

Mr Kelvin O'Reilly
Chief Executive Officer
Karting Australia

6/27 Ford Road
Coomera QLD 4209

PO Box 1297
Oxenford QLD 4209

karting.net.au



Australian National Drag Racing Association

The Australian National Drag Racing Association Ltd (ANDRA) is the drag racing sanctioning body of Australia.

The organisation was created in 1973 from a drag racing-oriented faction of the Australian Hot Rod Federation.

Today ANDRA sanctions races throughout Australia and throughout the year at all levels, from Top Fuel to Junior Dragster, on everything from temporary regional tracks to international standard facilities in capital cities.

ANDRA and its members make drag racing a family friendly, safe sport.

Contact details:

AUSTRALIAN NATIONAL DRAG
RACING ASSOCIATION LIMITED.

11 McInnes Street
Ridleyton SA 5008

Ph: (08) 8271 5355

Email: info@andra.com.au

andra.com.au



Australian Unity is a mutual organisation that has been helping Australians thrive for more than 175 years in the areas of Health, Wealth and Living. As the Official Health Insurance Partner of Motorsport Australia, Australian Unity offers club members, licence holders and motorsport fans the opportunity to access great offers through Australian Unity health insurance.



Burson Automotive, employer of more than 1,500 people in more than 160 auto parts stores across Australia, joined forces with Motorsport Australia in 2015. The partnership focuses on growing participation in motorsport through initiatives such as the Club Development Fund and the Motorsport Australia State Future Star Award.



Famous Insurance Agency and Motorsport Australia formed an alliance in 2015 to better cater for the insurance needs of the Australian motorsport community. Famous Insurance is underwritten by RACQ Insurance Limited.



Gallagher, the world's fourth largest insurance broking and risk management company, is the official Insurance Brokering Partner of Motorsport Australia. Providing specialist insurance and risk management solutions to the sporting and leisure industries since 1979, Gallagher passes the benefit of this experience to Motorsport Australia, its members and affiliates.



InWolk is a global IT consulting and product engineering services provider. As the official technology partner of Motorsport Australia, InWolk has continued to develop Motorsport Australia's internal systems along with the new look member portal, increasing functionality and usability to Motorsport Australia members.



Morris Finance is Australian owned and one of the nation's leading Finance Companies. Further to Morris' partnership with Motorsport Australia, Morris Finance also features as a Presenting Partner of the Motorsport Australia Academy program.



Motum Simulation and Motorsport Australia teamed up in 2018 to host the first ever Australian Esports round, at the Shannons Nationals. The Official Simulator Partner of Motorsport Australia provides pro-level driving simulators, used by top tier Australian and International drivers.



Skoop Printing partnered with Motorsport Australia in 2016 to manage the organisation's print needs. Skoop Printing provides tailored online systems for business, with the aim of reducing costs via appropriate sourcing and operational involvement.



S-Trend is Motorsport Australia's official merchandise and uniform partner. S-Trend's experienced and professional team work with their clients across and by working with teams across 55 different sporting codes to create high-quality, custom made branded merchandise, uniforms and team wear.

Occupational Health and Safety Policy

Scope

This policy applies to all Motorsport Australia staff and volunteers.



Policy Objective

This Policy shows Motorsport Australia's commitment to ensuring the health and safety of staff, contractors and volunteers who work for Motorsport Australia, and for minimising the risk to competitors, participants, officials, contractors, and visitors at our events, to the extent reasonably practicable.

Policy Statement

1. The Working Environment

Motorsport Australia will use its best endeavours to achieve a working environment that, to the extent reasonably practicable, eliminates or reduces risks to health and safety by:

- conducting risk assessments on hazards and risks relating to the work of Staff and Volunteers;
- monitoring the health and safety of Staff and Volunteers;
- seeking advice on safety matters when required;
- promoting safety and welfare to Staff and Volunteers;
- working with Staff who are returning to work after illness or injury to assist their rehabilitation;
- providing training to Staff on work health and safety on induction and then as required;
- providing training and instruction to Volunteers as to the safe performance of their work as required;
- ensuring contractors comply with their health and safety obligations by requiring them in Motorsport Australia contracts to address risks and have in place control measures to eliminate or reduce risks arising from their work;

- ensuring those conducting Permitted Events adopt any work health and safety policies required by Motorsport Australia and address risks and have in place control measures to eliminate or reduce risks arising from their Permitted Event/s;
- consulting with Staff and Volunteers on work health and safety matters;
- reporting to the Board Members on work health and safety.

2. The Motorsport Environment

Motorsport Australia recognises that motorsport may present risks to the health and safety of competitors, officials, contractors, and visitors at events. Motorsport Australia seeks to reduce those potential risks by:

- having a health and safety program ('the program'), which will protect and enhance the health and safety of all relevant stakeholders;
- having Board Members, Staff, Volunteers and Motorsport Australia-associated and affiliated organisations working together to develop and implement the program;
- providing training to Volunteers as required;
- applying the program at all Motorsport Australia events and, to the extent possible, Permitted Events;
- reviewing the program annually or more frequently if required;
- reporting on compliance and implementation to the Board Members.

3. Procedures

3.1 Best Practice

Motorsport Australia aims to be recognised as the motorsport leader in safety as it relates to its Staff and Volunteers.

3.2 Leadership

All Motorsport Australia managers and those in leadership positions, including senior volunteers, will provide the leadership needed to reach these goals.

3.3 Managers and Senior Volunteers

Motorsport Australia Managers and Senior Volunteers are committed to the provision and maintenance of a healthy and safe workplace, and to the extent reasonably practicable and as appropriate, will:

- consult and participate with Motorsport Australia Staff, Volunteers and associated stakeholders in the health and safety program;
- use risk identification, assessment and control principles to reach health and safety objectives;
- inform and train Staff and Volunteers and associated stakeholders in relevant policies, procedures and health and safety obligations;
- participate in Motorsport Australia induction and implement all safety procedures;
- allocate appropriate resources for the program.

3.4 Staff and Volunteers

Motorsport Australia Staff and Volunteers will:

- participate and support Motorsport Australia in its efforts to reach its health, safety and where relevant, rehabilitation objectives;
- follow reasonable health and safety instructions from managers, supervisors or senior volunteers;
- report any serious incidents, accidents, injuries or hazards in the workplace to supervisors or designated representatives;
- aim to work in a way that does not endanger the safety of themselves and/or others;
- properly use and maintain safety equipment;
- make sure other Staff and Volunteers and visitors follow safety rules in the workplace;
- participate in all Motorsport Australia induction programs when required, and follow all safety procedures.

3.5 Motorsport Australia Staff and Appointed Officials at Permitted Events

- Staff and Appointed Officials will conduct themselves in a safe manner and in line with local safety policies and procedures when attending Permitted Events on behalf of Motorsport Australia. In the absence of these, Motorsport Australia's organisational procedures will apply;
- Staff and Appointed Officials have the right to cease work at any Permitted Event if the staff member or Appointed Official has concerns that risks to health and safety have not been appropriately eliminated or reduced.

3.6 Consultation

Motorsport Australia consults on work health and safety with its Staff, Volunteers and stakeholders in a number of different ways and in accordance with any Consultation Policy.

3.7 Dispute Resolution

Any disputes as to work health and safety matters that are not addressed by other Motorsport Australia policies or procedures will be at the first instance addressed through informal mediation between the parties. If this is unsuccessful the Motorsport Australia CEO will refer the matter to formal mediation, and may then arbitrate the dispute if required.

Definitions

The following definitions apply to this policy:

Staff

A paid employee of Motorsport Australia who conducts work for the organisation including at Motorsport Australia Events and Permitted Events.

Motorsport Australia Appointed Officials

Officials appointed by Motorsport Australia to work at a Motorsport Australia Event or a Permitted Event, namely Stewards, Race Directors, Technical Commissioners and any other official expressly appointed by Motorsport Australia.

Contractor

A contractor or subcontractor engaged by Motorsport Australia.

Senior Volunteers

A volunteer who is a Board Member of Motorsport Australia, or a Chair of any Motorsport Australia Commission, Committee, State Executive, Working Group, Panel or is otherwise appointed by Motorsport Australia to a senior voluntary position.

Volunteers

Unpaid/Honorary representatives working for Motorsport Australia on authorised and approved business, including senior volunteers, Commission and Committee members and Motorsport Australia Appointed Officials.

Board Members (The Board)

Directors of the Motorsport Australia Board, which also includes the President and FIA delegate.

Motorsport Australia Events

Motorsport events conducted by Motorsport Australia, excluding Permitted Events.

Permitted Events

Motorsport events that are not conducted by Motorsport Australia but are conducted pursuant to a licence, permit or authority issued by Motorsport Australia.

Note: OH&S Policies on specific issues

Policies, procedures, operational safety requirements and safe work methods on specific occupational health and safety issues, consistent with the principles in this policy, will be issued as appropriate.



2019 Victorian Hill Climb - Rob Roy
Photo: Fifi welsh



2019 Adelaide Hills Rally
Photo: Wishart Media



2019 Motorsport Australia Motorkhana Championship
Photo: Andrew Sargeant



2019 Make Smoking History Targa West
Photo: CMR Photographic



2019 Bathurst 1000
Photo: Revved Photography



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NATIONAL COMPETITION RULES (NCR)

The National Competition Rules (NCR) are those specifically numbered in this section of this Manual and are recognised as the main body of law. They are supplemented by other items in the Manual, generally known as Appendices to the NCR.

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KEEP UP TO DATE ONLINE

To ensure you are equipped with the latest Motorsport Australia regulations, visit motorsport.org.au/regulations/manual.

The online Motorsport Australia Manual is the most up to date source of Motorsport Australia motorsport rules and requirements, with amendments to the Manual clearly marked in the online PDF's.

PART I – GENERAL PRINCIPLES

1. INTERNATIONAL CONTROL OF AUTOMOBILE COMPETITIONS

The Fédération Internationale de l'Automobile, hereafter termed the FIA, is the sole international sporting authority entitled to make and enforce regulations for the encouragement and control of automobile competitions and records and to organise FIA International Championships, and shall be the final international court of appeal for the settlement of disputes arising therefrom; it being acknowledged that the Fédération Internationale Motocycliste shall exercise the same powers insofar as vehicles with one, two and three wheels are concerned. For matters relating to Karting, the FIA may on an annual basis delegate its international sporting power to any recognised body, the role of which is to supervise international Karting activities of a sporting nature in strict accordance with this International Sporting Code and the FIA regulations.

2. INTERNATIONAL SPORTING CODE

So that the above powers may be exercised in a fair and equitable manner, the FIA has drawn up the present International Sporting Code ("ISC"). The purpose of the ISC and its appendices is to encourage and facilitate international motorsport. It will never be enforced so as to prevent or impede a competition or the participation of a competitor, save where the FIA concludes that this is necessary for the safe, fair or orderly conduct of motorsport. The ISC provides that:

- (i) each National Club (ASN – see Definitions) belonging to the FIA shall be presumed to accept and be bound by the ISC;
- (ii) subject to such acceptance, such ASN shall be recognised by the FIA as the sole body having power to control automobile competitions in its own country, Dominions, Dependencies, Protectorates and Colonies in conformity with the ISC; provided that if any Dominion, Dependency, Protectorate or Colony is represented directly on the FIA, the control of motorsport within that territory shall be exercised by the National Club of such territory;
- (iii) an ASN shall have the right to delegate the whole or part of the powers granted by the FIA to one or more clubs in its country, provided the consent of the FIA is first obtained, and to revoke such delegation if it notifies the FIA; and
- (iv) an ASN may draw up its own National Competition Rules (NCR), which will compulsorily be sent to the FIA. Such NCR must be within the parameters of the ISC. Pending approval by the FIA the Rules may be provisionally enforced forthwith.

3. NATIONAL CONTROL OF COMPETITIONS

- (i) Motorsport Australia, having been delegated power to control the sport in Australia by the FIA, and having acquiesced in and declared itself to be bound by the Statutes of the FIA and the ISC, now therefore declares its sole right, as a National Sporting Authority (ASN), to control automobile competitions in accordance with the ISC throughout the Commonwealth of Australia, its Protectorates and Dependencies, such territories being hereafter collectively referred to as the "territory of Motorsport Australia".
- (ii) For any Competition of a National or National Other status, open to the Motorsport Australia formulae and categories or groups such as defined by Motorsport Australia, each Automobile participating must comply in all respects with the Motorsport Australia technical regulations, and the official clarifications and interpretations of these regulations provided by Motorsport Australia. These Motorsport Australia technical regulations must not be modified without specific written permission from Motorsport Australia.

4. NATIONAL COMPETITION RULES

- (i) So that the above powers may be exercised in a fair and equitable manner, Motorsport Australia has formulated the National Competition Rules, which are in conformity with the ISC.
- (ii) The NCR shall include each appendix, section, schedule, code, regulation or policy which shall have the same authority as the NCR. The Board of Motorsport Australia may delegate the control and/or authority for modification to any of the above.
- (iii) It should be noted that the texts of the Supercars Australia Pty Ltd Operations Manual (which contractually must comply with the present ISC) contain certain modifications to the NCR which are applicable solely to Supercar competition. Since these modifications are not included in the NCR, it is specified that it is the current Supercars Australia Pty Ltd Operations Manual which is the authority text should there be differences between it and the present NCR.

5. APPLICATION OF THESE RULES

The NCR shall govern all automobile competitions organised under the authority of Motorsport Australia.

5a. OBJECT OF THESE RULES

- (i) The object of the NCR is to ensure that the conduct and promotion of motorsport in Australia is carried on in a manner which secures and enhances the safety of spectators, officials and competitors and which encourages the sport to be competitive and fair.
- (ii) To give effect to that object, the NCR shall not be administered or implemented for the purpose of:
 - (a) eliminating or substantially damaging a person who is a competitor of Motorsport Australia in a market in the event that Motorsport Australia is competing in a market at any relevant time;
 - (b) preventing the entry of a person into a market; or
 - (c) deterring or preventing a person from engaging in competitive conduct in a market.

providing always that the NCR shall not operate to prevent the imposition of an appropriate sanction or penalty for a contravention of any existing provision of the FIA Statutes, ISC or Regulations or Motorsport Australia's NCR for conduct arising hereafter, or for any amendment to any of those Rules provided the amendment is for the purpose of giving effect to the object in paragraph (i) above.

PART II – NOMENCLATURE, DEFINITIONS AND ABBREVIATIONS

6. DEFINITIONS – GENERAL

- (i) The following nomenclature, definitions and abbreviations shall be adopted in the NCR and the Schedules and Appendices thereto, in all Supplementary Regulations and for general use.

ACN: National Automobile Club. There is one per country, which embraces the whole of the national territory and covers road traffic, touring, motorsport and the interests and safety of participants. Its sporting authority must be authorised by the FIA.

AKA: Australian Karting Association Inc., to which Motorsport Australia has delegated control of all Sprintkart competition.

AMSAC: The Australian Motor Sport Appeal Court: the final motorsport appeal tribunal for Australian nationals.

ANDRA: The Australian National Drag Racing Association Ltd. to which Motorsport Australia has delegated control of all drag racing competition.

ASN: National Sporting Authority (Autorité Sportif Nationale). A national automobile club or other national body recognised by the FIA as sole holder of sporting power in a country.

BOARD: Means the Board of Motorsport Australia.

CIK-FIA (Commission Internationale de Karting): The organisation granted the power by the FIA to manage international matters concerning the sport of karting.

COMPETITION: A competition is an event in which an automobile takes part and which has a competitive nature or is given a competitive nature by the publication of results.

COURT OF THE FIRST INSTANCE: A judicial body which conducts an initial hearing and whose deliberations are not dependant on any earlier hearing of the same matter.

EVENT: An Event may include one or more competitions, which may comprise practice and qualifying sessions, heat/s and a final or may be divided in some similar manner, but must be completed by the end of the Event. An event is considered to have begun as from the time scheduled for the beginning of administrative checking and/or scrutiny and shall include practice and the competition itself. It shall end upon the expiry of one or other of the following time limits, whichever is the later:

- (a) time limit for protests or appeals or the end of any hearings; or
- (b) end of administrative checking and post-event scrutiny carried out in accordance with the NCR.

For any event, national or international, open to the FIA formulae and categories or groups such as defined in the present ISC and its Appendices, each automobile participating in the event must comply in all respects with the FIA technical regulations and with any official clarifications and interpretations of these regulations provided by the FIA. Motorsport Australia may not modify these FIA technical regulations without specific written permission from the FIA.

FIA: Federation Internationale de l'Automobile.

FIM: Federation Internationale Motorcycliste.

force majeure: Circumstances over which organisers, competitors or drivers have no control and which involve the impracticality of the competition continuing.

ISC: International Sporting Code of the FIA.

MANUAL: Motorsport Australia Manual, the Official Year Book of Motorsport Australia.

MOTORSPORT AUSTRALIA: The Confederation of Australian Motor Sport Ltd. trading as Motorsport Australia, the Board of Motorsport Australia and any other person, commission, committee or body within Motorsport Australia which holds an authorised delegation of power from the Board of Motorsport Australia. Motorsport Australia is the ASN for Australia and its territories.

NCR: National Competition Rule/s.

NRC: National Rally Code.

parc fermé: An area secured by the Organiser for the purpose of isolating automobiles. Each parc fermé shall be of sufficient size to accommodate all automobiles which are to be isolated. It shall be clearly delineated and controlled by officials appointed for that purpose so that only authorised persons can gain access. No repairs, replenishments, servicing or other work may be carried out on any competing automobile within parc fermé unless explicitly provided for and then only under the supervision of the officials.

ultra vires: going beyond the legal power of an official or tribunal.

- (ii) (i) In the NCR, the Appendices thereto, Supplementary Regulations, and in general use, unless the context otherwise requires:
 - (a) words importing the singular include the plural and vice versa;
 - (b) words importing any gender include the other genders;
 - (c) references to a person include corporations and bodies politic;
 - (d) references to a person include the legal personal representatives, successors and permitted assigns of that person;
 - (e) a reference to a function includes a reference to a power, authority and duty; and
 - (f) a reference to the exercise of a function includes where the function is a power, authority or duty, a reference to the exercise of the power or authority or the performance of the duty.

7. CLUB

- (i) **Motorsport Australia:** The national automobile club recognised by the FIA.

- (ii) **Affiliated club:** A body engaged in motoring activities and affiliated by Motorsport Australia as a club. To become affiliated, a club shall be required to comply with the requirements of Motorsport Australia as stated from time to time. Motorsport Australia reserves the right to grant, defer or refuse affiliation without stating any reason. Each club shall be classified in one of the following two categories:
 - (a) **Voting club:** An affiliated club which, at the discretion of Motorsport Australia, has voting powers on the relevant State Council of Motorsport Australia. In the event of membership of such a club, as shown in the annual declaration of membership, falling below 40 for two consecutive years, such club shall be reduced to non-voting status until such time as it again reaches 40, in any one year, in which case it shall forthwith be re-classified.
 - (b) **Non-voting club:** An affiliated club which has the right to be represented and to debate at Motorsport Australia State Council meetings, but has no voting power.
 - (c) **Enthusiast Club:** Each club affiliated with Motorsport Australia as an Enthusiast Car Club shall be considered only as a non-voting car club at all times.

8. ASSOCIATED BODY

An association, federation, club or group of clubs which does not organise motorsport events and whose members are generally members of Motorsport Australia, and which organisation has an affinity with Motorsport Australia. Membership of such an organisation shall not bestow upon its members membership of Motorsport Australia.

9. RECOGNISED BODY

An association, federation or other group of clubs etc., engaged in motoring activities and/or which has an integral role in the conduct of motorsport and to which Motorsport Australia may assign control of a part of motorsport.

10. DELETED 2007

11. MEMBERS OF MOTORSPORT AUSTRALIA

The following persons and clubs shall be deemed to be members of Motorsport Australia for the periods stated and shall be required to pay to Motorsport Australia such fee for this membership as may be decided by Motorsport Australia from time to time:

- (a) **Category A Member:** A financial member of an affiliated club, whether an individual or an organisation. A Category A Member (unless also a current Category E Member) is not entitled to notice of General Meetings nor to attend or vote or debate at General Meetings.
- (b) **Category B Member:** A Club which is affiliated with Motorsport Australia by the decision of the Board and which may be represented at State Council meetings by a nominee nominated from time to time in writing by that Club. Motorsport Australia shall be entitled to rely on such nomination as being conclusive of that nominee's standing. The nominee shall have the right to be present, debate, move and second motions and vote at State Council meetings only. A Category B Member (or their nominee) is not entitled to notice of General Meetings or to attend or vote or debate at General Meetings.
- (c) **Category C Member:** A Club which is affiliated with Motorsport Australia by the decision of the Board and which may be represented at State Council meetings by a nominee nominated from time to time in writing by that Club. Motorsport Australia shall be entitled to rely on such nomination as being conclusive of that nominee's standing. The nominee shall have the right to be present, debate and move and second motions at State Council meetings only, but shall have no voting rights. A Category C Member (or their nominee) is not entitled to notice of General Meetings or to attend or vote or debate at the General Meetings.
- (d) **Category D Member:** Each person determined from time to time by the Board to be a Life Member, Member of Honour or the holder of the Award of Merit. A Category D Member (unless they are also a current Category E Member) is not entitled to notice of General Meetings or to attend or debate or vote at General Meetings.
- (e) **Category E Member:** Each Director. A Category E Member has the right to notice of General Meetings and to be present, debate, move and second motions and vote at General Meetings.
- (f) **Category O Members:** All persons who volunteer to be officials and are licenced by Motorsport Australia to be officials at Motorsport Australia events. Category O Members (unless they are also currently Category E Members) are not entitled to notice of General Meetings or to attend or vote or debate at General Meetings.

Provided, however, that a person under a sentence of suspension or disqualification shall not be entitled to be a member of Motorsport Australia during the period of such suspension or disqualification. Fees previously paid by or in respect of such person shall not be refundable by Motorsport Australia in the event of such suspension or disqualification.

12. LAND VEHICLE, AUTOMOBILE, SPECIAL VEHICLE, GROUND EFFECT VEHICLE

- (i) **Land Vehicle:** A vehicle propelled by its own means in constant contact with the ground either directly by mechanical means or indirectly by ground effect, the motive power and steering system of which are constantly and entirely controlled by a driver on board the vehicle.
- (ii) **Automobile (includes car, vehicle, truck, superkart, buggy and utility/ute):** A land vehicle propelled by its own means, running on at least four wheels not aligned, which are designed to be in contact with the ground. The steering must be ensured by at least two of the wheels, and the propulsion by at least two of the wheels.
- (iii) **Special Vehicle:** A land vehicle on at least four wheels which is propelled otherwise than through its wheels.
- (iv) **Ground Effect Vehicle:** A land vehicle whose bearing on the ground is maintained by means of a pressurised air cushion.

13. CYLINDER CAPACITY

- (i) **Nominal Cylinder Capacity:**
 - (a) **Reciprocating engine (spark ignition and compression ignition):** The volume generated in the cylinder/s by the stroke of the piston/s. This volume is expressed in cubic centimetres and for all calculations relating to cylinder capacity the value of the symbol π shall be 3.1416.
 - (b) **Rotary Combustion (Wankel-type) engine:** The nominal engine capacity will be calculated by the following formula: 1.8 times the volume determined by subtracting the minimum capacity of the working chamber/s from its/ their maximum capacity, unless otherwise specified in Group technical regulations.
- (ii) **Supercharger:** refer Definitions – Technical.
- (iii) **Effective Cylinder Capacity (Spark Ignition Engine):** Unless specifically noted otherwise, where the spark ignition engine of an automobile includes a supercharger, the nominal cylinder capacity shall be multiplied by a factor of 1.7 and the automobile shall be classified for all purposes by the effective cylinder capacity thus obtained.
- (iv) **Effective Cylinder Capacity (Compression Ignition Engine):** Unless specifically noted otherwise, where the compression ignition engine of an automobile includes a supercharger, the nominal cylinder capacity shall be multiplied by a factor of 1.5 and the automobile shall be classified for all purposes by the effective cylinder capacity thus obtained.

14. CLASSIFICATION

Grouping of automobiles according to their engine cylinder capacity or by any other means of distinction.

15. COMPETITION

- (i) A competition shall be of one of the following kinds:
- International:** a competition which is open to competitors and/or drivers holding an FIA international licence;
 - National:** a competition which:
 - has been entered on a Motorsport Australia calendar;
 - is open only to the holders of the relevant licence issued by or acceptable to Motorsport Australia;
 - may be a Motorsport Australia Series or Championship or a round thereof or any other competition requiring a licence above the minimum requirement to compete in a Club competition;

Note: For the purposes of classifying events, including the setting of Permit Fees and the licensing of competitors and officials, the subdivisions within 'National' competitions shall be the same as applied in 2008/2009, and as are set out in Appendix R (www.motorsport.org.au), viz.:

- National
- National Other
- State
- Historic

Or

- Club:** a competition open only to the members of the organising club or of any explicitly named invited club and who hold the relevant Motorsport Australia licence. A Club competition may not be nor form part of any Motorsport Australia Series or Championship.

Note: For the purposes of classifying events, including the setting of Permit Fees and the licensing of competitors and officials, the subdivisions within 'Club' competitions shall be the same as applied in 2008/2009, and as are set out in Appendix R (www.motorsport.org.au), viz.:

- Multi-Club
- Club

- Restricted Competition:** Any competition may be restricted by the requirement that in order to participate drivers, competitors or automobiles be required to comply with specified particular qualifications.
- Motorsport Australia may, in special circumstances, vary the applicable status level of a competition up or down from what would generally be expected from the specifications above. This may be for the application of permit conditions or fees, for the application of safety conditions, or for some other reason considered appropriate.
- An Event made up of a number of competitions will normally take its status level from the highest status competition at the Event. Competitions within the Event may have lower status levels applied to them for the purpose of licences, conditions or application of other regulations.

16. RALLY

A road event with an imposed average speed, which is run entirely or partly on roads open to normal traffic. A "rally" consists either of a single itinerary which must be followed by all automobiles, or of several itineraries converging on a same rallying-point fixed beforehand, possibly followed by a common itinerary. The route may include one or several special stages, i.e. events organised on roads closed to normal traffic, and which together determine the general classification of the rally. The itineraries which are not used for special stages are called road sections. Speed must never constitute a factor determining the classification on these road sections.

17. OBSERVED SECTION TRIAL

- A competition comprising a number of specified sections, each of no more than 200 metres, and over difficult terrain (e.g., of wet, muddy, rough, tortuous and/or steep nature). Such sections shall be designated or shown adequately to the participating crews in advance and shall be marked by signs or flags and, if necessary, guidelines.
- The competition shall be untimed and shall be determined solely by the ability of each competing automobile to maintain forward movement within the confines of the designated course.

18. MOTORKHANA

A competition designed to test the acceleration, braking and general manoeuvrability of the automobiles and the skill and judgement of the crew members. A motorkhana may be conducted on a sealed or an unsealed surface and shall not be, nor include, a speed event. No straight section of any test may exceed 60 metres in length without a turn of at least 90 degrees. Only one automobile is allowed on each test at any one time. Each Motorkhana Test must be contained within an area of 100 metres x 50 metres, including any garages.

18A. KHANACROSS

A single-car competition conducted on sealed or unsealed surfaces or a combination of both and involving a series of timed tests on various layouts. Each shall be designed to test the acceleration, braking and general manoeuvrability of the vehicle and the skill and judgement of the driver. No straight section of a course shall exceed 100 metres in length without a turn of at least 90°. The course for each test shall be clearly marked. Up to two competing cars may be permitted on each test at the same time, subject to approval of Motorsport Australia. Khanacross is not a speed event.

18B. DRIFTING

A speed event competition conducted on a sealed surface wherein the object is to produce as much yaw rate as possible from the automobile whilst demonstrating the driver's ability to control the direction and speed of the automobile around a pre-determined course. Drifting is not determined by time.

19. TOURING ASSEMBLY

A competition organised with the sole object of assembling participants at a point settled beforehand. In a Touring Assembly a prescribed route may have to be followed, and each participant may have to report at prearranged points, but no intermediate limits of speed between start and final assembly may be laid down. The program of a Touring Assembly may include additional events (other than speed). No prize money or prizes of monetary value (other than trophies) may be allocated in respect of a Touring Assembly, and the award of, or offer to award, any such prize money or prizes of monetary value other than trophies, certificates etc., is not permitted.

19A. TOURING ROAD EVENT

- (i) A competition which consists of a series of road, transport and/or navigation sections whose itinerary and regulations are designed so that all relevant road laws can be complied with by each competitor, together with at least one special test. Special tests may include:
 - (a) speed tests/events;
 - (b) closed road tests; and
 - (c) skill tests (e.g., motorkhana, navigation, khanacross, auto-test, average speed)
- (ii) Other forms of tests may be carried out in conjunction with the road sections; e.g. economy tests, observation tests, regularity tests.

Note: at least one special test must be included in the itinerary, otherwise the event is either a rally or a touring assembly and must comply with the regulations for those events.

19B. NAVIGATION ASSEMBLY

- (i) A competition, in which participants use navigation to arrive at a point/s determined beforehand and which may include additional competition/s (other than any Speed Event, Race, Rally or Off Road event).
- (ii) Conditions for a Navigation Assembly shall comply with the National Touring Code.
- (iii) Each Crew member may be required to report at a prescribed point/s.
- (iv) A time for the closure of the Major Control at the end of each Division shall be set. No intermediate limit of time within a Division may be set.
- (v) No award of a prize valued at over \$500.00 may be allocated in respect of a Navigation Assembly.

20. DELETED 2007

20A. PARADE

A display of a group of automobiles at a moderate speed and must be approved by Motorsport Australia. The following conditions must be observed:

- (a) an official car shall lead the parade and another shall close it;
- (b) each of these official cars shall be driven by an experienced driver who shall be under the control of the Clerk of the Course;
- (c) overtaking is strictly forbidden;
- (d) timing is forbidden; and
- (e) the parade must be included in the Supplementary Regulations.

20B. DEMONSTRATION

A display of an automobile's performance which must be approved by Motorsport Australia. The following conditions apply:

- (a) may be controlled by an "Official Car" driven ahead of the field by an experienced driver who shall be under the control of the Clerk of the Course;
- (b) each service required for the relevant competition must be provided;
- (c) each driver/co-driver/navigator must wear safety apparel as required for the relevant competition;
- (d) each automobile must pass scrutiny for safety;
- (e) only a driver, co-driver or navigator may occupy the automobile;
- (f) overtaking is strictly forbidden unless under the instructions of marshals showing blue flags;
- (g) timing is forbidden; and
- (h) the demonstration must be included in the Supplementary Regulations.

20C. REGULARITY TRIAL

A multi-car speed event in which each competitor nominates the driver's target time beforehand and in which the results are determined on the basis of variation from that time.

20D. CHAMPIONSHIP, CUP, TROPHY, CHALLENGE OR SERIES

A Championship, Cup, Trophy, Challenge or Series may comprise a single event or a number of events with common regulations, subject to the approval of Motorsport Australia.

20E. COMMERCIAL NAMING RIGHTS

The right to associate the name of a commercial company, organisation or brand with a Motorsport Australia Championship, Cup, Trophy, Challenge, or Series is subject to the prior approval of Motorsport Australia.

21. CERTIFIED TRIAL

A performance achieved under the supervision of officials appointed by Motorsport Australia in accordance with the regulations for Certified Trials as approved by Motorsport Australia.

22. SPEED EVENT

A competition other than a race in which each automobile is timed or judged individually along a course greater than 200m in length and which is determined solely by time and/or any penalties incurred. A speed event shall include a record attempt, Hill Climb, Rallycross, Lap Dash/Sprint, Super Sprint, Autocross, Regularity Trial, Drifting and a speed or acceleration test by whatever name, and any other activities as determined by Motorsport Australia.

23. RACE

- (i) **Circuit Race:** a competition held on a closed circuit between two or more automobiles, running at the same time on the same course, in which the result is determined either by the order in which the automobiles cross the finish line after completing the specified number of laps, or by the distance covered in the specified time.
- (ii) **Drag Race:** a competition between two automobiles racing from a standing start over a straight precisely measured course in which the first automobile to cross the finish line (without penalty) achieves the better performance.

24. AUTOCROSS

A speed event conducted on a predominately unsealed licensed course which must not exceed 4km in length. Any track longer than that distance shall be subject of a review and exemption by Motorsport Australia.

24A. HILL CLIMB

A single-car speed event in which each car covers the same course. The finish line shall be situated at a greater altitude than the start line, unless the track licence provides otherwise. More than one car may be permitted on the course at the same time, subject to the approval of Motorsport Australia.

24B. SUPER SPRINT

A multi-car speed event conducted on a circuit in which vehicles are started in accordance with the Speed Event Standing Regulations. The maximum number of vehicles permitted on the track at any one time shall be noted on the Track Licence. Overtaking is permitted but "racing" is not. The results shall be determined on the basis of elapsed time for each vehicle including any penalties incurred.

25. OFF ROAD EVENT

A competition conducted on a course over a variety of terrains and which may be defined as "short course" (up to 15km) or "long course" (15km or greater).

26. RALLYCROSS

A speed event conducted on a combination of sealed and unsealed surfaces as part of a permanent circuit. Each competing automobile shall be separately timed, the winner being determined by the lowest individual time recorded in one event or the lowest aggregate time over a pre-determined number of heats.

26A. OTHER COMPETITION

As specifically approved by Motorsport Australia.

27. PROMOTER AND/OR ORGANISER

- (i) Except where the Promoter is also the Organiser, the Promoter of an event shall be a person or body with responsibility for financial and commercial matters only. A Promoter who is not also the Organiser shall not intervene during an Event in respect of matters covered by the NCR.
- (ii) The Organiser is the person or body having responsibility for the organisation of an Event including but not limited to technical and sporting matters in accordance with the NCR.

28. ORGANISING PERMIT

A document issued by Motorsport Australia authorising the organisation of a competition under the NCR.

29. ORGANISING COMMITTEE

The Organiser shall appoint an Organising Committee which shall consist of at least three persons invested with all necessary powers for the organisation of the Event and the enforcement of Supplementary Regulations save that for club Events, the Organising Committee may consist of fewer persons.

30. SUPPLEMENTARY REGULATIONS

A compulsory official document approved by Motorsport Australia and issued by the Organiser of a sporting competition with the object of specifying the details of the competition.

31. PROGRAM

An official document prepared by the Organiser of an Event which includes details of the competition. A program is obligatory for National events and above.

32. COURSE

The route to be followed by an automobile in a competition.

33. TRACK

A permanent or temporary course used for competitions or record attempts.

34. SPEEDWAY

A permanent circuit, with not more than four corners, each of which turns in the same direction.

35. METRIC CONVERSION

- (i) For all conversions of imperial to metric measurements and vice versa, the mile shall be taken as 1.609344 kilometres, and the kilometre shall be taken as 0.62137 miles.
- (ii) In cases where exact measurement is not vital (e.g., length of tracks other than for records etc.), distances may be measured to the nearest whole metre.
- (iii) In cases of attempts at records, the measurements shall be as specified by Motorsport Australia.
- (iv) In cases where a close to exact measurement is required, it shall be necessary to record measurements to within 0.025mm (e.g., measurements of bore, stroke etc.).

36. RECORD

The best result obtained in particular conditions prescribed by the regulations.

- (i) **National Record:** A record established in conformity with the NCR within the territory of Motorsport Australia, or within the territory of another ASN with the prior authorisation of the latter. A national record shall be a class record or an absolute record.
- (ii) **World Record, Absolute World Record, Outright World Record:** Refer to the ISC

37. HOLDER OF RECORD

If the record is one established in the course of an individual attempt, the holder is the competitor to whom permission to make the attempt was granted and who made formal application for such permission. If the record is one established in the course of an Event, the holder is the competitor in whose name the record-breaking automobile was entered.

38. THE START

The start is the moment when permission to start is given to a driver or to several drivers starting together.

39. CONTROL LINE

A line at the crossing of which by an automobile, timing or other performance criteria are determined.

40. STARTING LINE

The first control line of a competition course.

41. FINISHING LINE

The final control line of a competition course.

42. HANDICAP

The method laid down in the Supplementary Regulations with the object of equalising the results of a competition.

43. COMPETITOR

A person or body who holds a competitor's licence acceptable to Motorsport Australia and has entered a competition.

43A. AUTHORITY TO COMPETE

Where the owner of the automobile entered in an event is not the competitor, the competitor must produce written consent from the owner authorising the use of the automobile in the competition.

44. DRIVER (INCLUDES CO-DRIVER)

A person holding a licence acceptable to Motorsport Australia and entered to drive an automobile in a competition.

45. NAVIGATOR

A person, other than a driver, holding a licence acceptable to Motorsport Australia, carried in an automobile during a competition, whose function is to navigate the automobile along a prescribed route.

46. PASSENGER

A person, other than the driver and any navigator, carried in an automobile during a competition or other activity permitted by Motorsport Australia. Any passenger shall be permitted in a race or speed event only by the written permission of the CEO of Motorsport Australia.

47. 47. MOTORSPORT AUSTRALIA LICENCE

(i) Competition Licence:

- (a) A competition licence is a certificate of registration issued by Motorsport Australia to an eligible member or body and which is necessary to enter and/or participate in a competition. If issued to an individual it is valid only when held in conjunction with current membership of an affiliated club (other than any affiliated Enthusiast Club).
- (b) Such licence shall entitle the holder to participate only in competitions in accordance with the limitations attaching to the particular licence.
- (c) The holder of a Motorsport Australia licence is deemed to know and be bound by the NCR.
- (d) An FIA international licence issued by Motorsport Australia or another ASN is valid for international competitions in accordance with the ISC.
- (e) A competition licence issued by another ASN is acceptable for Australian national competitions as provided by any border agreement approved by the FIA.
- (f) The benefits and privileges of a Motorsport Australia licence are available to the holder only at activities conducted under the NCR and opportunities offered by Motorsport Australia.

(ii) Official's Licence:

- (a) A licence issued to a person who is accredited as an official by Motorsport Australia;
- (b) The holder of a Motorsport Australia Official's Licence is deemed to know and be bound by the NCR, the Motorsport Australia Officials and Accreditation Guide and the terms and conditions of the Licence itself; and
- (c) A Motorsport Australia Official's Licence is granted on condition of continuing compliance with its requirements and may be cancelled, suspended and/or withdrawn at any time by Motorsport Australia.

48. CLUB MEMBERSHIP

Any person competing in a competition must be a current member of a Motorsport Australia-affiliated club other than any affiliated Enthusiast Club or be the holder of a competition licence issued by another ASN which is acceptable for Australian national competitions as provided by any border agreement approved by the FIA.

49. DELETED 1999

50. LICENCE NUMBER

The number allotted by Motorsport Australia to the holder of a licence, which is entered on a register.

51. LICENCE HOLDERS' REGISTER

The list held by Motorsport Australia of licence holders.

52. NATIONALITY

A competitor or driver who holds a licence issued by Motorsport Australia is deemed to be an Australian national for the period of validity of that licence. Irrespective of the nationality of their licence, each competitor or driver participating in any FIA World Championship event shall retain the nationality of their passport in all official documents, meetings, information bulletins and prize-giving ceremonies.

53. TRACK LICENCE

- (i) A track licence is a certificate of registration issued by Motorsport Australia for tracks in accordance with Appendix T, as follows:
 - (a) **Category A:** For tracks on which International races may be held;
 - (b) **Category B:** For tracks on which competitions, other than International race events, may be held;
 - (c) **Category C:** For tracks on which competitions other than races may be held.
- (ii) Nothing in this NCR shall be deemed to prevent the holding on Category A and B tracks of lesser events than those for which their category qualifies them, as above.
- (iii) Category A and B Licences must be issued by Motorsport Australia National Office.
- (iv) Category C Licences may be issued by either Motorsport Australia National or State Offices.

54. ORGANISATION OF A COMPETITION

A competition may be organised in the territory of Motorsport Australia only by:

- (i) Motorsport Australia
- (ii) an affiliated club (other than any affiliated Enthusiast Club); or
- (iii) another person or body approved by Motorsport Australia.

55. OFFICIAL DOCUMENTS

For each competition, the Organiser shall compile and, with the prior approval of Motorsport Australia, publish Supplementary Regulations and, for a National Event, a Program. If any condition contained in these official documents is contrary to the NCR, it shall be null and void.

56. ANNOUNCEMENT TO BE MADE ON ALL OFFICIAL DOCUMENTS

All Supplementary Regulations, program and entry forms relating to any competition shall bear in a conspicuous manner the following announcement:

"Held under the FIA International Sporting Code including Appendices and the National Competition Rules of Motorsport Australia."

57. ACQUAINTANCE WITH AND SUBMISSION TO RULES

- (i) Each person or group of persons organising a competition or taking part therein, including but not limited to competitors, officials, category managers, promoter, organiser and circuit operator:
 - (a) shall be deemed to be acquainted with the Constitution of Motorsport Australia and the NCR;
 - (b) shall undertake to submit themselves without reserve to the above and to the decisions of Motorsport Australia and to the consequences resulting therefrom; and
 - (c) acknowledges that the NCR are:
 - (A) made in the best interests of motorsport in Australia;
 - (B) necessary and reasonable for the purpose of protecting and promoting motorsport;
 - (C) necessary and reasonable for the purpose of establishing safety regulations; and
 - (D) made in the public interest.
- (ii) Each person, body or group of persons acknowledges and agrees that this clause may be pleaded as an absolute bar to proceedings, suit or action against Motorsport Australia; and
 - (a) agrees that they will not become a party to any suit, at law or equity, against Motorsport Australia, its officials or any other persons subject to the NCR, unless and until all remedies allowed by the Constitution of Motorsport Australia and the NCR have been exhausted, save with the written consent of Motorsport Australia; and
 - (b) in case of non-compliance with these provisions, any person or group which organises a competition or takes part therein, may have any Motorsport Australia licence which has been issued to them withdrawn.

58. UNSAFE COMPETITIONS

- (i) The organisation or holding of any automobile competition other than in accordance with the NCR shall render each person connected therewith or taking part therein, whether as Promoter, Organiser, Organising Committee, competitor, driver, official, or otherwise, liable to the consequences and penalties provided by the NCR. If such a competition is included in an Event for which an organising permit has been granted, the permit shall be null and void, and all entry fees received from any competitor at such Event shall be returned to the competitor.
- (ii) Each person connected with the holding of any automobile competition other than in accordance with the NCR acknowledges that the effect of the NCR is to restrict the ability of that person to compete or participate in, promote, organise or otherwise hold or provide services at an authorised competition.

59. POSTPONEMENT, ABANDONMENT OR CANCELLATION OF A COMPETITION

- (i) An Event or a competition forming part of an Event shall not be postponed, abandoned or cancelled unless provision for doing so has been made in the Supplementary Regulations, or unless the Stewards have decided to postpone or abandon it for reasons of force majeure or safety.
- (ii) In case of cancellation, postponement for more than 24 hours or abandonment, entry fees shall be returned within 30 days of the date of cancellation unless the competitor states their intention of competing in a postponed competition.
- (iii) In the case of any Road Event or Off Road Event having been cancelled due to force majeure, the body issuing the permit may, upon application by the Organiser, permit such Organiser to retain up to 25 per cent of the entry fees.

60. DELETED 2007

PART IV – PRELIMINARY ORGANISATION OF COMPETITION

61. DELETED 2007

62. RESPECT OF LAWS AND REGULATIONS

- (i) A competition may be held on a road or on a track, or on both, but no permit shall be granted by Motorsport Australia unless the Organiser undertakes to obtain the necessary permissions from the relevant local authorities.
- (ii) Occupational Health and Safety legislation imposes obligations on the Organiser to ensure the conduct of the Event, including the management of contractors, is in accordance with applicable legislation.
- (iii) A competition taking place on public roads open to normal traffic must conform to the relevant Road Traffic Regulations of the State/s in which it is conducted. Penalties for any violation of such regulations shall be determined by the Stewards.

63. NECESSARY ORGANISING PERMIT

No competition, demonstration, parade or other competition activity shall be held unless Motorsport Australia has issued an organising permit. Motorsport Australia may attach conditions to the grant of a permit or decline to grant or may withdraw a permit without stating a reason.

64. APPLICATION FOR AN ORGANISING PERMIT

Each application for an organising permit shall be lodged with Motorsport Australia at least one month prior to the date of the proposed Event, with the following:

- (a) the name and address of the Promoter and, if different, the name and address of the Organiser;
- (b) the names of the persons proposed to form the Organising Committee and the address of this committee;
- (c) a draft of the Supplementary Regulations applicable to each competition of the Event; and
- (d) where applicable the permit fee.

65. PERMIT FEES

- (i) The scale of fees payable to Motorsport Australia (or to the FIA through Motorsport Australia) on the application for an organising permit shall be as prescribed in Appendix R to the NCR (refer www.motorsport.org.au).
- (ii) The Chief Executive Officer may vary the permit fee or vary the payment schedule for an individual competition or activity. Any variation may be revoked by Motorsport Australia without a reason being given.

66. DELETED 2007

67. INFORMATION TO BE INCLUDED IN SUPPLEMENTARY REGULATIONS

- (i) The Promoter and the Organiser;
- (ii) the name, nature and definition of each proposed competition;
- (iii) a statement in accordance with NCR 56;
- (iv) the composition and address of the Organising Committee;
- (v) the names of the Stewards and of the Clerk of the Course;
- (vi) the place and date of the Event;
- (vii) where appropriate, reference to the Trans-Tasman Agreement;
- (viii) a full description of each proposed competition (length and direction of the course, classes and categories of automobiles admitted, fuel, restriction on the number of competitors [if such limitation exists], etc.);
- (ix) all necessary information concerning entries (lodgement details, dates and times of opening and closing, entry fee, etc.);
- (x) eligibility of competitors and drivers and the licence requirements including civil licences if appropriate;
- (xi) the maximum number of entries and how entries if in excess of this number are to be selected for acceptance;
- (xii) details as to the conditional acceptance of entries;
- (xiii) conditions under which entries may be refused;
- (xiv) all relevant information concerning insurance;
- (xv) the dates, times and types of starts, with indication of handicaps, if any;
- (xvi) the manner in which the results will be determined and prizes awarded;
- (xvii) a detailed list of awards offered for each competition;
- (xviii) a reminder of the NCR concerning protests;
- (xix) the provisions regarding postponement, abandonment or cancellation;
- (xx) whether the nomination of drivers may be delayed;
- (xxi) for all circuit races, the words "and the Circuit Race Standing Regulations" and for other events a statement validating the relevant Standing and/or Sporting Regulations and/or discipline code;
- (xxii) the Judges of Fact and the facts to be judged. For National competitions those judges shall be named;
- (xxiii) the times, places and dates fixed for the examination of automobiles;
- (xxiv) details of documents to be presented at document verification including:
 - (a) current competition licence/s;
 - (b) current Motorsport Australia-affiliated car club membership card; and
 - (c) Motorsport Australia Vehicle Log Book; and
- (xxv) all such other requirements and information as may be necessary for the proper conduct of the Event.

68. FUEL AT AN EVENT

Unless explicitly approved by Motorsport Australia, no limitation on the brand of fuel at an Event shall be permissible, and any Supplementary Regulation purporting to limit competitors or drivers to fuel of any one brand shall be null and void.

69. AMENDMENT OF SUPPLEMENTARY REGULATIONS

- (i) An amendment shall only be made to the Supplementary Regulations after the beginning of the period for receiving entries with the agreement of each competitor already entered in that competition and with the written approval of Motorsport Australia, or by a decision of the Stewards for reasons of exceptional circumstances, of force majeure or of safety.
- (ii) An Organiser may issue a Further Regulation after the publication of Supplementary Regulations. A Further Regulation shall form part of the event regulations and each competitor is bound by it.
- (iii) No document, including Supplementary Regulations, Further Regulations or written instructions to competitors and/or drivers shall be issued without the approval of Motorsport Australia.
- (iv) Where practical an Organiser shall require each competitor to acknowledge receipt of a Further Regulation or written instruction.

70. INFORMATION TO BE INCLUDED IN THE OFFICIAL PROGRAM

- (i) A statement that the Event is to be held in accordance with NCR 56;
- (ii) place and date of the Event;
- (iii) a short description including championships/series/cups/etc., and the timetable of the proposed competitions;
- (iv) names of competitors and drivers with the numbers allocated for their automobiles. If any pseudonyms are used, they must appear between inverted commas;
- (v) the name of the Promoter and of the Organiser;
- (vi) the events to which handicaps may apply;
- (vii) a detailed list of awards offered for each competition;
- (viii) the names of the Stewards and of the Clerk of the Course; and
- (ix) for events of National status and above, a "Motorsport Australia page" free of charge to Motorsport Australia; the contents thereof to be provided by Motorsport Australia.

70A. WORDING ON PASSES AND PROGRAMS

The current wording of disclaimers, which must appear on each pass and program, is available on the Motorsport Australia website, www.motorsport.org.au.

71. DELETED 2007

72. ENTRY

An entry is a contract between a competitor and the Organiser. It must be signed by the competitor and by each driver. It binds the competitor and the driver to make every effort to take part in the competition, except in case of force majeure; and binds the Organiser to fulfil all the conditions of the entry.

73. LEGAL OWNER

- (i)(i) Only the *bona fide* legal owner of an automobile shall be entitled to enter the automobile in any competition unless the Competitor at the time of entry produces the written consent of the *bona fide* legal owner to enter the automobile in the competition and who shall then for all purposes hold the status of the *bona fide* legal owner.
- (ii) The Competitor shall hold a valid competition licence as described in the General Regulations of Motorsport Australia.
- (iii) In the case of the *bona fide* legal owner being a company, it may by notice in writing authorise a person to enter the automobile in a competition on behalf of the company. In such case the company must be the holder of a Competitor licence.
- (iv) Any attempt to enter an automobile in any competition without a valid competition licence shall be a breach of this NCR and the entry or attempted entry shall be void. In such case the entry fee shall be forfeited to the Promoter of the competition.

74. DELETED 1999

75. DISPUTE REGARDING AN ENTRY

Any dispute between a competitor and the Organiser regarding an entry shall be determined by Motorsport Australia.

76. RECEIPT OF ENTRIES

- (i) Following the publication of approved Supplementary Regulations the Organiser may receive entries. Entries shall be made in writing on the Entry Form supplied by the Organiser or by electronic means.
- (ii) For an entry to be valid the entry fee must be received by the Organiser prior to the closing date of entries.
- (iii) In the absence of authorisation from the ASN which issued their licence/s, the entry of a foreign competitor and/or driver is null and void.

77. CONTENTS OF ENTRY FORM

The Entry Form for each competition shall contain the following:

- (a) spaces for the full name, address and licence number of the competitor, driver/s, navigator/s and approved passenger/s together with space for any pseudonyms;
- (b) statements in accordance with NCR 56 and 79 of these Rules;
- (c) a space for the signature and date of signing by each of the above individuals;
- (d) a space for the signature and date of signing by the appropriate parent or guardian of a competitor, driver, navigator or approved passenger who is under 18 years of age;
- (e) a space for the emergency contact details or next of kin of the competitor, driver/s, navigator/s and approved passenger/s;
- (f) such other information as is required by the NCR or by the Supplementary Regulations; and
- (g) spaces for the recording of:
 - (A) the date of receipt of such Entry Form by the Secretary of the Event;
 - (B) the date and time of official acceptance of the entry by the Organiser.

78. ENTRIES IN NATIONAL EVENTS BY FOREIGN LICENCE HOLDERS

With the prior approval of Motorsport Australia, a competitor, driver or navigator holding a licence issued by an ASN other than Motorsport Australia may be eligible to compete in a national or club competition subject to:

- (a) the provisions of the Trans-Tasman Agreement;
- (b) the holding of an Australian passport; or
- (c) meeting the requirements of the ISC and, if an historic competition, FIA Appendix K.

79. UNDERTAKING BY COMPETITORS, DRIVERS, NAVIGATORS AND PASSENGERS

Each competitor, driver, navigator and passenger shall complete a disclaimer as available on the Motorsport Australia website, www.motorsport.org.au.

80. CLOSING OF ENTRIES

The date for the closing of entries must be specified in Supplementary Regulations. For International competitions the closing of entries shall be in accordance with the ISC; for National competitions not fewer than three days prior to commencement of the Event; and for Club competitions, not later than the commencement of competition.

81. ENTRY BY ELECTRONIC MEANS

An entry may be made by facsimile or by any other electronic means of communication provided it is received before the closing of entries. Payment may be made by any means acceptable to the Organiser provided it is received before the closing of entries.

82. ENTRY CONTAINING A FALSE STATEMENT

An entry which contains a false statement shall be null and void. Such action is a breach of the NCR in which case the entry fee may be forfeited to the Organiser.

83. REFUSAL OF ENTRY

It shall be the obligation of the Organiser to advise an intending competitor in writing whether or not their entry has been accepted as follows:

- (a) An entry for an International competition may be refused in accordance with the ISC;
- (b) an entry for a National competition may be refused in accordance with the Supplementary Regulations not later than two days after the closing date for entries;
- (c) an entry for a Club competition may be refused only prior to the commencement of the competition; and
- (d) an entry for a competition counting towards a Motorsport Australia championship or Motorsport Australia-recognised series shall not be refused unless such refusal has been approved by Motorsport Australia.

84. CONDITIONAL ACCEPTANCE OF ENTRY

Supplementary Regulations may provide that an entry will be accepted under specific conditions such as a limitation on the field size. A conditional acceptance must be notified to the competitor by letter or electronic means sent at the latest on the day following the closing of entries. A competitor whose entry is accepted conditionally is not subject to the provisions of NCR 145.

85. ACCEPTANCE OF ENTRIES

If the number of entries received exceeds the maximum field size, those to be accepted shall be selected in the manner specified in the Supplementary Regulations. If no manner is specified, they shall be selected either in the order of their receipt or by ballot as the Organiser shall decide. Acceptance or conditional acceptance of entries shall clearly state the time and date of such acceptance or conditional acceptance.

86. PUBLICATION OF ENTRIES

An Organiser may only list as a participant any competitor or driver for whom an entry from such competitor or driver has been accepted.

87. NOMINATION OF RESERVES

Should an entry be rejected in accordance with NCR 85, it may be accepted as a reserve subject to the prior agreement of the competitor.

88. ENTRY OF AN AUTOMOBILE

An automobile shall not be entered more than once in the same competition unless approved otherwise by Motorsport Australia.

89. INSTRUCTIONS TO COMPETITORS AND DRIVERS

Any instructions to competitors and drivers must not conflict with the provisions of the Supplementary Regulations.

90. INSURANCE

It shall be mandatory upon each Organiser and licence holder to comply with such requirements regarding insurance as prescribed by Motorsport Australia.

91. OFFICIAL LIST OF COMPETITORS

For any National Event, the Organiser must send to Motorsport Australia, and make available to each competitor at least two days prior to the commencement of the Event, the official list of competitors including drivers taking part. For a Club Event such list must be made available to each competitor prior to the commencement of the competition.

PART V – COMPETITION COURSES

92. COMPETITION COURSE

When the course of a competition traverses the territory of more than one State Council, the Organiser must first obtain approval from each relevant State Council.

93. ROAD COURSE

Any road course proposed for use in a competition shall be submitted to Motorsport Australia for approval together with a detailed itinerary indicating the relevant distances and times.

94. MEASUREMENT OF A COMPETITION COURSE

For a competition other than a record attempt, distances up to five kilometres shall be measured along the centreline of the road by a qualified surveyor. Distances greater than five kilometres shall be determined by the official road markings or by means of an official map to a scale not less than 1:50,000.

95. INTERNATIONAL TRACK OR SPEEDWAY LICENCE

An application for an international track or speedway licence shall be made to Motorsport Australia in accordance with the ISC.

96. NATIONAL TRACK LICENCE

Motorsport Australia may issue a National Track Licence for a track for a specific period. Motorsport Australia may refuse to issue or may withdraw a National Track Licence as it may think fit and without giving any reason. A national track shall not be used for an international competition nor for an attempt on an international record.

97. INFORMATION TO BE GIVEN ON A TRACK LICENCE

Each Motorsport Australia Track Licence will state the length of the track and whether it is approved for attempts at national records. The licence shall also contain a schedule showing the maximum number of automobiles permitted to start in any competition.

98. DISPLAY OF A TRACK LICENCE

The current Track Licence shall be displayed in a prominent position at each licensed track.

99. CONDITIONS TO BE FULFILLED FOR PERMANENT AND TEMPORARY TRACKS

Each track whether permanent or temporary shall comply with such conditions and requirements as determined by Motorsport Australia.

100. TRACK LICENCE FEES

The scale of fees payable on the granting of a Track Licence shall be as prescribed by Motorsport Australia.

101. TYPES OF START

- (i) There are two types of start:
 - (a) rolling start; and
 - (b) standing start.
- (ii) A driver shall be deemed to have started at the moment when permission to start is given.

102. STARTING LINE

- (i) In a record attempt and in a competition with a rolling start, the starting line is the one which, when crossed, starts the timing of the automobile.
- (ii) In a competition with a standing start, the starting line is the line in relation to which the position of each automobile is fixed, prior to the start.
- (iii) In starting from the pit lane:
 - (a) where the automobile is positioned forward of the Starting Line the automobile will be considered as having completed one lap, the first time it crosses the Control Line; and
 - (b) where the automobile is positioned behind the Starting Line the automobile will be considered as having completed one lap, the second time it crosses the Control Line.

103. STARTING POSITIONS

Supplementary Regulations shall define the relative positions of each automobile at the start of each competition and the method by which each position is determined.

104. ROLLING START

A rolling start occurs when the automobile is moving at the moment when the timing commences. For a rolling start, automobiles may be led by an official car prior to the starting signal. The competition will have started when the leading automobile crosses the starting line having received the start signal.

105. STANDING START

- (i) A standing start occurs when the automobile is stationary at the moment when the start signal is given.
- (ii) For automobiles starting singly or in line abreast:
 - (a) If timing is by automatic timing equipment, each automobile shall be placed in accordance with the Supplementary Regulations;
 - (b) If timing is by other than by automatic timing equipment, each automobile shall be placed with the contact patch of the front wheels on or behind the starting line.
- (iii) For a grid start, the timing shall commence when the signal to start is given.
- (iv) In a competition on a closed circuit, timing shall occur on each subsequent lap as each automobile crosses the control line.
- (v) After the publication of the final starting grid, the places of any non-starters shall remain empty, the other automobiles retaining their published positions.
- (vi) For a record attempt with a standing start, the automobile must be stationary with that part which operates the timing not more than 100mm behind the starting line. The engine of the automobile shall be running before the start.

106. TIMING OF ROAD EVENTS

Timing procedures for Road Events shall be prescribed in the Supplementary Regulations.

107. DELETED 2007

108. CROSSING A CONTROL LINE

The timing of an automobile crossing a control line shall be taken at the moment when the foremost portion of the automobile passes over that line or, where automatic timing equipment is used, at the moment when it is operated.

109. STARTER'S ORDERS

The Starter's Orders shall be as prescribed in the Standing Regulations for the discipline.

110. NON-STARTER

An automobile not coming under starter's orders shall be deemed a non-starter.

111. FALSE START

In a circuit race a false start occurs when an automobile under starter's orders is outside its prescribed position or is moving when the starting signal is given.

112. PENALTY FOR A FALSE START

- (i) A driver who makes a false start shall be penalised in accordance with the Supplementary Regulations. This penalty shall be notified to the competitor as soon as practicable.
- (ii) The Stewards shall have the power to increase any specified penalty and/or to apply other penalties.

113. HEAT

A competition may be conducted in heats, the format of which must be defined in the Supplementary Regulations. The composition of the heats may be modified only with the approval of the Stewards.

114. DEAD HEAT

In the case of a dead heat the competitors concerned shall share the combined awards including points and any prizes allotted to the places they have collectively filled in the competition.

115. INTERRUPTED COMPETITION

Nothing in these rules shall prevent the resumption of a competition which has been interrupted by force majeure or by the use of the red flag.

115A. DELETED 2007

116. FINISHER

To be classified as a finisher in a competition an automobile must cross the finishing line under its own power.

PART VII – RECORDS

117. JURISDICTION

For World Records, Absolute World Records and the Outright World Record, refer to the ISC. Motorsport Australia shall submit to the FIA a claim for any of the above records attempted in its territory.

118. ELIIGIBILITY TO ESTABLISH RECORDS

A National record shall be established only with an automobile complying with the NCR.

119. RECORDS RECOGNISED

- (i) Motorsport Australia may recognise a National record. A record may be recognised in more than one classification.
- (ii) No distinction is made between records set on a track or road.

120. DELETED 2013

121. CONDITIONS FOR RECORDS

Motorsport Australia shall establish the conditions, including duration and/or distance, for each National record.

122. RECORDS ESTABLISHED DURING AN EVENT

- (i) No time or distance record established during a race or speed event shall be recognised.
- (ii) A Lap Record can only be established during a race.

123. DELETED 2013

124. DELETED 2013

125. REGISTER OF RECORDS

- (i) Motorsport Australia shall maintain a register of each National record.

126. PUBLICATION OF RECORDS

- (i) Pending the recognition of a record, the result of the attempt shall not be advertised unless the words "Subject to confirmation" appear in clearly legible characters. A breach of this requirement may result in the non-recognition of the claim without prejudice to other penalties.

127. DELETED 2007

PART VIII – COMPETITORS AND DRIVERS

128. REGISTRATION OF COMPETITOR AND DRIVER

Each person participating in a competition, whether as competitor, driver, navigator or as a crew member aboard a competing automobile, must comply with the Motorsport Australia licensing requirements.

129. MISLEADING OR FALSE INFORMATION

An omission or the making of a false statement on a licence application shall be deemed an offence under the NCR.

130-139. LICENCES

Motorsport Australia's licence regulations are referred to in "General Regulations of Motorsport Australia" in the Motorsport Australia Manual. Refer to the Motorsport Australia website www.motorsport.org.au

140. MEDICAL - FITNESS

Each driver and navigator participating in a competition must comply with the conditions of medical fitness which apply to their competition licence.

141. SAFETY EQUIPMENT – APPAREL

Each occupant of an automobile must comply with the requirements of apparel for that competition. Any non-compliant apparel may be impounded by the Organiser for the duration of that Event.

142. PSEUDONYM (ASSUMED NAME)

- (i) If a licence is desired under a pseudonym a specific application shall be made to Motorsport Australia.
- (ii) The licence holder, for so long as they are registered under that pseudonym, shall take part in competition only under that pseudonym.
- (iii) An alteration of a pseudonym shall necessitate the same procedure being followed as for the original application.

143. CHANGE OF DRIVER

In a competition other than an attempt at a record, a change of driver shall be made only if the Supplementary Regulations allow. After publication of the program, such change shall require the approval of the Stewards.

144. RESPONSIBILITY OF COMPETITOR AND DRIVER

- (i) Each competitor shall be responsible for all acts or omissions on the part of any person connected to their entry.
- (ii) Each such person or body shall be jointly and severally liable for any breach of the NCR.
- (iii) Each competitor must ensure that their automobile complies with the conditions of eligibility and safety throughout the competition.
- (iv) The presentation of an automobile for scrutiny is deemed to be an implicit statement of conformity to the relevant regulations.

145. COMPETITOR AND DRIVER FORBIDDEN TO ABANDON ONE COMPETITION AND COMPETE IN ANOTHER

- (i) Any competitor having entered or any driver having undertaken to drive in any competition, who does not take part in that

- competition and who takes part in another competition on the same day at some other place shall be suspended as from the beginning of the latter competition, for such time as Motorsport Australia may deem fit.
- (ii) If the two competitions take place in different countries, the provisions of the ISC shall apply.

145A. ANTI-DOPING POLICY

The control of prohibited substances is regulated by the Motorsport Australia Anti-Doping Policy. In any areas where the NCR and the Anti-Doping Policy conflict the Anti-Doping Policy will apply.

145B. ILLICIT DRUGS IN SPORT POLICY

The control of illicit substances is regulated by the Motorsport Australia Illicit Drugs in Sport (IDIS) Policy. In any areas where the NCR and the IDIS Policy conflict the IDIS Policy will apply.

PART IX – AUTOMOBILES IN COMPETITION

146. PRESCRIPTION OF AUTOMOBILES

Each automobile used in competition or in an attempt on a National or Local record shall comply with the prescriptions of automobiles as set down in "General Requirements for Cars and Drivers" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au.

147. MARKINGS ON AN AUTOMOBILE

Each automobile shall display such markings and numbers as prescribed in Schedule K (see "General Requirements for Cars and Drivers" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au) and the Supplementary Regulations for the competition.

148. DANGEROUS CONSTRUCTION

The Stewards shall disqualify any automobile the construction of which is deemed to be dangerous.

149. PROTECTION AGAINST FIRE

Each automobile shall be constructed and equipped so as to comply with the specific requirements for fire protection.

150. LOG BOOK

Each automobile in competition shall be the subject of a log book in accordance with Schedule L (see "General Requirements for Cars and Drivers" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).

151. REGISTRATION

Motorsport Australia may require registration of an automobile.

152. SUSPENSION OR EXCLUSION OF A PARTICULAR AUTOMOBILE

- (i) Motorsport Australia may suspend, exclude or otherwise penalise a particular automobile as a result of a breach of the NCR by the competitor or the driver, by the manufacturer, or by any person or body holding the official agency for selling and/or servicing the automobile.
- (ii) For international penalties, refer to the ISC.

153. SUSPENSION OR EXCLUSION OF A MAKE OF AUTOMOBILE

Motorsport Australia may suspend or exclude a make of automobile for a breach of the NCR by the manufacturer, by their accredited representative, or by any person or body holding the official agency for selling and/or servicing the make concerned. If Motorsport Australia wishes the suspension of a make of automobile to apply internationally, or if it is desired to exclude a make of automobile, it shall forward such request to the FIA in accordance with the ISC.

154. DELETED 2007

155. ADVERTISEMENTS ON AN AUTOMOBILE

- (i) Advertising may be displayed on an automobile in accordance with Schedule K (see "General Requirements for Cars and Drivers" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au) and the Supplementary Regulations for the competition.
- (ii) Any sign or advertisement which is deemed to be indecent or in bad taste shall not be permitted.
- (iii) Any sign or advertisement which is in breach of any Government Legislation shall not be permitted, except whereby an exemption is granted by Motorsport Australia.

156. ADVERTISING AND PUBLICITY OF COMPETITIONS

- (i) Any person or body advertising or publicising a competition or the results of any competition is required to do so in a manner which is not likely to mislead the public.
- (ii) Any advertisement or announcement relating to the results of a competition shall clearly state the exact conditions of the performance referred to, the nature of the competition, the category, class etc. of the automobile, the position and result obtained, if applicable, that results are "subject to official confirmation by Motorsport Australia", and such other information as Motorsport Australia may require.
- (iii) Any breach of this rule may entail a penalty on any manufacturer, automobile, competitor, driver, or other Motorsport Australia licence holder responsible for publishing the advertising.

157. NAMING OF AN AUTOMOBILE

- (i) The name of an automobile, as recorded in the log book, is subject to the approval of Motorsport Australia.
- (ii) Each automobile shall be known by and entered in competition under the name shown in the log book.

158. EXAMINATION OF AN AUTOMOBILE

- (i) At any Club event where separate competitions (including practice) occur on more than one day, each automobile shall be examined for compliance prior to commencement of competition on each day.
- (ii) For any other event, examination for compliance shall be conducted prior to commencement of competition, unless Targeted Scrutiny prescriptions apply.
- (iii) At any time during an event an automobile may be examined to ascertain its compliance.
- (iv) Where a seal has been placed on an automobile by an official it is forbidden under pain of disqualification to tamper with such seal, or to remove such part save under the authority of an authorised official.

- (v) **Automobile eligibility:** The Chief Scrutineer at the Event, having noted an area of minor ineligibility, may complete a statement:
"In my view, the minor ineligibility noted in the log book of this automobile does not improve the performance to such an extent that the automobile should be disqualified from this Event."
 Each log book bearing such an endorsement by the Chief Scrutineer shall be placed before the Stewards. If the Stewards approve such a log book endorsement for a minor ineligibility during the Event, then the automobile shall not be regarded as ineligible in respect of the item/s noted and no protest on that ground by any person shall be accepted in respect of that Event.
- (vi) An endorsement for a specific minor ineligibility in the log book is valid only for the Event at which it was granted. The defect must be corrected for the automobile to be eligible for any future competition.
- (vii) Motorsport Australia may impound for examination any automobile for which a log book has been issued or which has been used in competition.
- (viii) Motorsport Australia may conduct a post event inspection at a time and place determined by Motorsport Australia. The details of any such inspection shall be advised to the competitor and each component nominated for inspection shall be sealed or impounded.

159. ELIGIBILITY OF AUTOMOBILES

- (i) Motorsport Australia may publish regulations which establish the prescriptions governing the eligibility of an automobile for a specific competition.
- (ii) Motorsport Australia in its sole discretion shall determine whether an automobile or any or all components thereof, including fuels, lubricants and other consumable items, and/or practices which may influence the performance of the automobile comply with the regulations for any competition, class or group. A competitor may seek a written determination by the Motorsport Australia Eligibility Committee (MAEC) in which case such determination is binding in any proceeding. The MAEC shall have the absolute authority of Motorsport Australia to make any determination or decision for the purposes of this NCR.
- (iii) The MAEC shall consider matters as follows:
 - (a) In the case of an eligibility dispute arising at or from a competition, the Stewards or other judicial body may either determine the issue themselves or refer it to the MAEC for determination.
 - (b) Eligibility matters not arising from a competition (as in (iii) (a) above) shall be referred to the MAEC for determination. This process is available only to:
 - (A) the holder of a competitor licence which entitles them to enter the automobile, for which a log book is current, in a competition; or
 - (B) the CEO of Motorsport Australia; or
 - (C) a Technical Delegate.
- (iv) Any determination or decision of the MAEC shall be unconditionally binding on any hearing by the Stewards, a Tribunal or the AMSAC.
- (v) Otherwise, and subject to the agreement of the MAEC Chairman, a member of Motorsport Australia may seek an opinion from the MAEC. Such opinion shall not be binding in any further relevant issue.
- (vi) Notwithstanding the above, any determination regarding the initial acceptance of an automobile for historic classification, including the production and issue of a Certificate of Description, Automobile or Vehicle Specification Sheet or Log Book for an automobile of the 5th Category shall be in compliance with the General Regulations for 5th Category - Historic Automobiles.

PART X – OFFICIALS AND THEIR DUTIES

160. LIST OF OFFICIALS

The term "official" may include but is not limited to the following persons, who may have assistants:

- (a) Stewards;
- (b) Race Director;
- (c) Clerk of the Course;
- (d) Secretary of the Event;
- (e) Compliance Checker;
- (f) Chief Medical Officer;
- (g) Chief Timekeeper;
- (h) Technical Delegate;
- (i) Chief Scrutineer;
- (j) Observer (e.g., pit, track control);
- (k) Marshal (e.g., track, flag control);
- (l) Judge of Fact (e.g., start, finish);
- (m) Chief Starter; and
- (n) Handicapper.

161. RIGHT OF OBSERVATION

Motorsport Australia may appoint any person to observe any aspect of a competition.

162. ESSENTIAL OFFICIALS

At each Event there shall be unless otherwise specified in the discipline regulations contained in the Motorsport Australia Manual:

- (a) at least two Stewards;
 - (b) a Clerk of the Course;
 - (c) a Secretary of the Event; and
 - (d) a Chief Scrutineer;
- and, in the case of competitions decided wholly or partly by time:
- (e) a Chief Timekeeper;
- and, at circuit race Competitions:
- (f) a Medical Officer;
- each of whom shall be an Essential Official.

163. NOMINATION OF OFFICIALS

The Stewards and other specific officials shall be appointed by Motorsport Australia. Each other official of the Event shall be appointed by the Organiser and must hold the appropriate Official's licence.

164. DELETED 2007

165. PLURALITY OF DUTIES

At an Event any official appointed by the Organiser may undertake multiple duties provided they are licensed for each, save for the Compliance Checker who, at a National Event or above, shall have no other role.

166. SEPARATION OF DUTIES

- (i) An official shall not, at any Event, perform any duties other than those for which they are appointed.
- (ii) An Essential Official shall not compete in any competition at which they are appointed as an official.
- (iii) An Essential Official who has any direct beneficial connection with a competitor or driver shall stand aside from their appointment.

167. STEWARDS TO BE HONORARY

The Stewards shall act in an honorary capacity.

168. DUTIES OF THE STEWARDS

- (i) The Stewards are appointed by Motorsport Australia to officiate as a panel under the Chair of the Chief Steward. They shall be responsible for establishing agendas and ensuring that minutes of all meetings are produced.
- (ii) In the event of a split ballot among the Stewards, the Chair shall have a casting vote.
- (iii) The Stewards shall sign and send to Motorsport Australia a closing report as soon as practicable after the close of the Event. This report shall include their comments on the conduct of the Event and the results of each competition together with particulars of any protests lodged, decisions made, penalties applied, notices of Intention to Appeal received and any recommendations in regard to a suspension or a disqualification and any information provided by the Clerk of the Course.
- (iv) In an Event comprising several competitions, there may be different Stewards for each competition.

169. AUTHORITY OF THE STEWARDS

- (i) The Stewards shall have supreme authority for the enforcement of the Supplementary Regulations including the ISC, NCR, sporting and any other relevant regulations. In particular, the Stewards shall have authority to:
 - (a) settle any issue which might arise during an Event, subject to the right of appeal in accordance with the NCR;
 - (b) decide what penalty to apply in the event of a breach of the regulations;
 - (c) in exceptional circumstances, amend the Supplementary Regulations;
 - (d) in events conducted in heats, alter the composition or the number of heats;
 - (e) in the event of a dead heat, authorise a resolution in accordance with the NCR;
 - (f) authorise a change of driver;
 - (g) overrule a decision of, or accept or refuse any correction from, a Judge of Fact;
 - (h) impose penalties;
 - (i) amend the results in accordance with the NCR;
 - (j) disqualify from competing any automobile deemed to be unsafe and any driver deemed to be dangerous or medically unfit;
 - (k) disqualify from a competition or from the Event any competitor, driver or automobile which is found to be ineligible;
 - (l) disqualify any competitor, driver or official who they determine to be guilty of improper conduct or unfair practice;
 - (m) direct the removal, on pain of disqualification, of any sign or advertisement that they deem to be in bad taste; such decision being final and not subject to appeal;
 - (n) order the removal from the course and its precincts of any competitor or driver who refuses to obey the order of an official;
 - (o) postpone a competition in the case of force majeure or for serious safety reasons;
 - (p) modify the program and the course in consultation with the Clerk of the Course or the Organiser in the interests of safety;
 - (q) ensure there are at least two Stewards, if necessary by the other Steward/s or Motorsport Australia appointing a replacement;
 - (r) approve the commencement of the competition/s; and
 - (s) in extraordinary circumstances, take the decision to stop an event or other course activity.
- (ii) For club/multi-club events where Stewards are not appointed the separation of duties listed within this NCR may be allocated as follows:
 - (a) decisions may be taken by the Clerk of the Course on the following articles and any protest is to be transmitted to the Duty Officer for a subsequent Stewards Hearings – Article 169. (i) - (a), (c), (e), (g), (h), (i), (k), (l), (p);
 - (b) decisions may be taken by the Clerk of the Course on the following articles – Article 169. (i) - (f), (j), (m), (n), (o), (r), (s);
 - (c) decisions may be taken by the Clerk of the Course on sporting issues on the following article, with any judicial/disciplinary issues refer to the Duty Officer for transmission to a Stewards Hearing – Article 169. (i) - (b).

170. DELETED 2007

171. POWERS OF MOTORSPORT AUSTRALIA FOLLOWING STEWARDS' REPORT

If it appears from the Stewards' Report or otherwise that the conduct of an Event was not in accordance with the NCR, or that the results of a competition have been improperly or incorrectly made out, Motorsport Australia shall be entitled to inquire into the matter and may make such determinations and orders as it sees fit. No such inquiry shall be ordered after the expiration of 60 days after the last day of sporting activity or publication of the results, whichever is later.

172. DUTIES OF THE CLERK OF THE COURSE

- (i) The Clerk of the Course is responsible for conducting the Event in accordance with the Supplementary Regulations.
- (ii) In the case of an Event comprising several competitions there may be a different Clerk of the Course for each competition.
- (iii) In particular the Clerk of the Course shall:
 - (a) keep order, in conjunction with the civil authorities who are responsible for public safety;

- (b) remain in close contact with the Stewards throughout the Event in order to ensure the smooth running of each event;
- (c) ensure that sufficient properly-qualified officials are present for the conduct of the Event and report any deficiencies to the Stewards;
- (d) obtain the approval of the Stewards to commence competition;
- (e) ensure that the necessary information and equipment is provided to each official to enable each to carry out their duties;
- (f) prevent any ineligible competitor, driver or automobile from taking part in a competition;
- (g) control each competitor, driver and automobile;
- (h) ensure that each automobile and, if necessary, each driver carries the proper identification markings;
- (i) ensure that each automobile is driven by the correct driver;
- (j) assemble the automobiles in such categories and classes as are required;
- (k) submit to the Stewards any proposal to modify the program, any improper conduct, breach of rule, or protest;
- (l) receive any protest and transmit it immediately to the Stewards;
- (m) collect the reports of the timekeepers, scrutineers, track or course marshals, and other information necessary for the determination of the results;
- (n) collect the reports of the Medical Officer for delivery to Stewards; and
- (o) submit promptly the closing report for the Event to the Stewards for their consideration and approval.

173. DUTIES OF A DIRECTOR – SERIES/RACE/EVENT/SPORTING

- (i) A Series/Race/Event/Sporting/etc. Director may be appointed by Motorsport Australia for a Championship, Series, Cup, Trophy, Challenge etc.
- (ii) Duties and responsibilities will be set down in the relevant Sporting Regulations.
- (iii) A Director may submit a report directly to the Stewards provided the Clerk of the Course is also informed.

174. DUTIES OF THE SECRETARY OF THE EVENT

The Secretary of the Event shall be appointed by the Organiser and be responsible for the planning of the Event with duties which will necessarily include:

- (a) publication of Supplementary Regulations;
- (b) receipt and processing of entries;
- (c) the recruitment and deployment of officials;
- (d) the supply of information and equipment to officials;
- (e) the publication to Motorsport Australia and to each competitor of provisional results within 120 hours of the completion of the competition or as otherwise specified by Motorsport Australia and subsequently the publication of the final results; and
- (f) in events where required, the submission of completed (planned) Safety 1st Checklists to Motorsport Australia prior to the issuing of a permit for the event and the submission of completed (operational) Safety 1st Checklists to the Stewards upon completion of the event.

175. DUTIES OF THE CHIEF MEDICAL OFFICER

The duties of the Chief Medical Officer shall include:

- (a) organisation and operation of the medical services;
- (b) assessment of the physical and psychological fitness of any driver ensuring through the Clerk of the Course that an unfit driver does not compete;
- (c) reporting to the Clerk of the Course any decisions made which may affect the organisation of the Event;
- (d) ensuring treatment of casualties;
- (e) directing the placement of medical services;
- (f) completion of Accident and Injury Reports for the Clerk of the Course; and
- (g) remaining in communication with the medical services and event control.

176. DUTIES OF THE CHIEF TIMEKEEPER

The duties of the Chief Timekeeper shall include:

- (a) use of timing equipment compliant with the discipline regulations;
- (b) declaration of times taken in order to compile the results;
- (c) preparation of signed reports to be sent to the Clerk of the Course accompanied by any necessary documents; or to Motorsport Australia in the case of an attempt at a record or a test;
- (d) sending, on their request, original time sheets either to the Stewards or to Motorsport Australia; and
- (e) communication of any times or results only to the Stewards and the Clerk of the Course or in accordance with their instructions.

177. DUTIES OF THE CHIEF SCRUTINEER

- (i) The Chief Scrutineer is responsible for determining compliance of each automobile and associated equipment including apparel with safety and technical regulations as specified in the Supplementary Regulations.
- (ii) The responsibilities of the Chief Scrutineer shall include:
 - (a) making inspections as requested by Motorsport Australia, the Organiser, the Stewards, the Clerk of the Course and a Technical Delegate before, during or after the Event;
 - (b) using such checking instruments as may be specified by Motorsport Australia;
 - (c) communicating any official information only to Motorsport Australia, the Organiser, the Stewards, the Clerk of the Course and a Technical Delegate; and
 - (d) preparing and signing a report for the Clerk of the Course.

178. DUTIES OF A TECHNICAL DELEGATE

A Technical Delegate is a national official appointed annually by Motorsport Australia under terms of reference determined by Motorsport Australia. A Technical Delegate shall report to the Chief Scrutineer, the Clerk of the Course and the Stewards who shall take appropriate action to prevent the participation of an ineligible automobile.

179. DUTIES OF PIT AND SERVICE MARSHALS

Pit and Service Marshals shall monitor all replenishment of automobiles during a competition and other activities as specified in the Supplementary Regulations. They shall report to the Clerk of the Course.

180. DUTIES OF OBSERVERS AND FLAG MARSHALS

- (i) Observers and Flag Marshals shall occupy posts assigned to them. When an Event commences, each is under the orders of the Clerk of the Course to whom each shall immediately report by the means at their disposal and confirmed in writing each incident or accident which occurs along the section to which they are assigned.
- (ii) Flag Marshals are specifically entrusted with flag signalling (see Appendix H in "Race" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).

181. DUTIES OF A JUDGE OF FACT

- (i) The Supplementary Regulations for any competition must indicate which facts are to be judged by the Judges of Fact.
- (ii) Where a decision is to be given whether an automobile or participant has touched or crossed a given line, or upon any other fact of the same type and which has been specified in the Supplementary Regulations, one or several Judges of Fact shall be nominated.
- (iii) For all circuit racing competitions, the name of each Judge of Fact must be available to each competitor and, unless included in the Supplementary Regulations, shall be displayed on any official notice board. Unless Motorsport Australia requires otherwise, for all other competitions the Supplementary Regulations shall indicate which facts are to be judged and the name of each Judge of Fact shall be made available by the Organiser on the request of a competitor.
- (iv) Each Judge of Fact shall report to the Clerk of the Course.

181A. DECISION REVIEW

- (i) No protest against a decision of a Judge of Fact shall be accepted concerning a matter which they have been appointed to judge.
- (ii) If a Judge of Fact considers that they have made a mistake they may correct it, subject to this correction being accepted by the Stewards.
- (iii) The Stewards may overrule a decision of a Judge of Fact and use any information to assist them in reaching a decision.

182. DUTIES OF THE HANDICAPPER

The handicapper shall, after entries have closed, prepare the handicaps in accordance with the requirements of the Supplementary Regulations. The Supplementary Regulations shall state if any handicap in a competition is to be amended as a result of a performance achieved in a previous competition.

182A. COMPLIANCE CHECKER

A Compliance Checker may be appointed by the Organiser of an event to assist the Secretary of the Event to undertake the requirements of the Safety 1st Checklists appropriate to the event.

PART XI – PENALTIES

183. BREACH OF NCR

- (i) In addition to any offences referred to elsewhere, the following are breaches of the NCR:
 - (a) any attempt direct or indirect to bribe any person having duties in relation to a competition or being employed in any manner in connection with a competition and the acceptance of, or offer to accept, any bribe by such an official or employee;
 - (b) any action having as its object the entry or participation in a competition of a person, body or automobile found to be ineligible therefore;
 - (c) any fraudulent conduct or any act prejudicial to the interests of any competition or to the interests of motorsport generally;
 - (d) knowingly giving false evidence;
 - (e) failing to comply with the directions of a Motorsport Australia judicial body;
 - (f) failing to pay to Motorsport Australia within one calendar month of their falling due any monies due to Motorsport Australia;
 - (g) any action not in accordance with the NCR;
 - (h) failing to follow the directions of Motorsport Australia or an official;
 - (i) any action which causes or is reasonably likely to cause damage to other persons or property;
 - (j) using abusive, insulting, threatening or obscene language, or behavior towards an Official;
 - (k) using or offering violence by any means towards any person;
 - (l) unauthorised removal of a seal; and
 - (m) any act prejudicial to the interests of motorsport generally.
- (ii) A person or entity may be charged with, and found guilty of, one or more breaches of NCR 183 and with other offences arising from the same action/s or omission/s.

184. PENALTIES FOR PARTICIPATION IN AN UNAUTHORISED COMPETITION

- (i) Any person or body who shall promote, enter for, drive in, officiate at, or in any manner whatsoever take part in, or advertise or obtain publicity for the results of, a competition not organised in accordance with the NCR, but which by its nature should, in the opinion of Motorsport Australia, have been held in accordance with the NCR; or who shall become excluded or suspended by the governing body of any other sport recognised by the FIA has committed a breach of the NCR and will be penalised in accordance with the NCR with such penalty and for such time as Motorsport Australia may think fit.
- (ii) Provided that where the prescribed competition has been or is to be held outside the jurisdiction of Motorsport Australia, Motorsport Australia and the other ACN concerned shall refer the matter to the FIA, whose decision thereon will be final.
- (iii) A person or body who breaches this NCR acknowledges that the breach is contrary to the best interests of motorsport and in particular is without proper regard to safety and public interest.

185. IMPOSITION OF PENALTIES

- (i) Any breach of the NCR may result in a penalty.
- (ii) Penalties may be imposed by Motorsport Australia and by its judicial bodies, and otherwise as provided in the NCR.
- (iii) Before imposing any penalty other than a time, pit lane or drive through penalty, the Stewards shall convene a hearing at which the defendant may state their case unless they choose not to exercise that right.

- (iv) At Appeal, AMSAC and other judicial hearings the rights of the appellant are determined by the court.
- (v) If the Stewards find that an offence has been committed which warrants a penalty outside their jurisdiction they shall refer the matter to a Disciplinary Tribunal whose sole task will be determination of penalty.
- (vi) Alleged offences arising out of a competition, but not dealt with by the Stewards, shall be dealt with by a Disciplinary Tribunal acting as a Court of First Instance.
- (vii) Alleged offences not arising out of a competition shall be dealt with by a judicial body acting as a Court of First Instance.
- (viii) Notwithstanding the provisions of this NCR, where an Organising Committee which makes the application for an organising permit fails to publish results of a competition within the time limits set down in the NCR it shall be subject to an administrative fine imposed by Motorsport Australia. Such penalty shall not be subject to appeal.

186. PENALTIES

- (i) Penalties which may be imposed are, in order of increasing severity:
 - (a) warning;
 - (b) reprimand;
 - (c) fine;
 - (d) obligation to accomplish some work of public interest;
 - (e) sundry penalty including time, starting position, finishing position, stop/go and pit lane drive-through;
 - (f) disqualification;
 - (g) suspension; and
 - (h) exclusion.
- (ii) Motorsport Australia may approve Supplementary or Series regulations which specify fixed and other penalties and their method of application. The quantum of a fixed penalty may not be varied in any circumstances.

187. REPRIMAND OR FINE

A reprimand or a fine may be imposed by the Stewards, by any Motorsport Australia tribunal, by a State Council, by Motorsport Australia, or by AMSAC provided that any fine imposed shall not exceed the relevant amount specified in Appendix R (refer www.motorsport.org.au).

188. LIABILITY TO PAY A FINE

- (i) A competitor shall be responsible for the payment of any fine imposed on their driver, team member, passenger etc.
- (ii) The Organiser shall be responsible for the payment of any fine imposed on their official/s.

189. TIME LIMIT FOR PAYMENT OF A FINE

- (i) A fine must be paid within two days of notification unless approved otherwise by the Stewards.
- (ii) Any delay in making payment will entail suspension during the period a fine remains unpaid.

190. ALLOCATION OF PROCEEDS FROM FINES

The proceeds from all fines including any monetary penalties, forfeited protest fees and appeal fees shall be remitted to Motorsport Australia and shall be used in accordance with the Board of Motorsport Australia policy.

191. DISQUALIFICATION

- (i) A sentence of disqualification may only be imposed by:
 - (a) the Stewards;
 - (b) a State Council;
 - (c) Motorsport Australia;
 - (d) a Motorsport Australia tribunal; or
 - (e) AMSAC
 and may be made retrospective in operation.
- (ii) A sentence of disqualification means the competitor, automobile or person so sentenced shall be removed from the results of and/or prohibited from further participation in one or more competitions at an Event. In each case, disqualification shall entail the forfeiture of the entry fee.
- (iii) An appeal against a sentence of disqualification shall not set aside such sentence pending determination.

192. TIME PENALTIES

A time penalty may be applied only for an infringement occurring during the relevant competition activity.

193. SUSPENSION

- (i) A sentence of suspension may be imposed only by:
 - (a) a State Council, a tribunal, AMSAC or the Board of Motorsport Australia;
 - (b) the CEO of Motorsport Australia (where the CEO is satisfied that there is a potential or real risk to health or safety); or
 - (c) as otherwise required or permitted by the NCR.
- (ii) A sentence of suspension shall entail the loss of any right to take part in any capacity in any competition held within the territory of Motorsport Australia or under any FIA-approved agreement involving another ASN.
- (iii) Suspension shall render null and void an entry for any competition which may take place during the term of such suspension.
- (iv) Any Motorsport Australia licence holder who is suspended shall return their licence to Motorsport Australia immediately. Any delay in returning a licence shall automatically result in the extension of the suspension by a period equal to the delay.
- (v) Motorsport Australia shall endorse any licence which has been suspended accordingly.
- (vi) When separate penalties of suspension are imposed they shall be cumulative in effect.
- (vii) International licence suspension shall be implemented in accordance with the ISC.
- (viii) A Motorsport Australia licence holder whose licence is suspended by the CEO of Motorsport Australia acting under NCR 193(i) may make a written submission to the Board of Motorsport Australia seeking a review of the suspension imposed by the CEO of Motorsport Australia. Following receipt of a submission, the Board of Motorsport Australia shall consider the suspension imposed by the CEO of Motorsport Australia and may, in its sole discretion, increase, reduce or cancel the suspension. During any such consideration, the suspension shall remain in effect notwithstanding the making of a written submission to the Board of Motorsport Australia.

194. INELIGIBLE AUTOMOBILE

When an automobile has been found to be ineligible the Stewards or other judicial body shall disqualify it from at least the relevant competition/s (e.g., practice, qualifying, heat, or race).

195. EXCLUSION

- (i) A sentence of exclusion shall result in the permanent loss of any right to take part in any capacity whatsoever in any competition.
- (ii) A sentence of exclusion can be imposed only by Motorsport Australia and will be reserved for offences of exceptional gravity.

196. EFFECTS OF EXCLUSION

- (i) A sentence of exclusion shall always be international in nature.
- (ii) Exclusion shall render null and void any entry in any competition.
- (iii) Notification of this penalty to the FIA shall be in accordance with the ISC.

197. STATEMENT OF REASONS FOR SUSPENSION OR EXCLUSION

In notifying a sentence of suspension or of exclusion to the FIA, Motorsport Australia shall give its reasons for imposing such penalty.

198. DELETED 2007**199. SUSPENSION OR EXCLUSION OF AN AUTOMOBILE**

A sentence of suspension or exclusion may be pronounced on either a particular automobile or on a make of automobile.

200. LOSS OF AWARD

Any licence holder disqualified, suspended or excluded during a competition shall lose the right to obtain any awards assigned to the said competition.

201. AMENDMENT OF PLACINGS AND AWARDS

In the event that a licence holder is disqualified from the results of a competition or Event, the placings and awards shall be amended, the following competitors being advanced in order.

202. PUBLICATION OF PENALTIES

Motorsport Australia shall have the right to publish or to have published that it has penalised a licence holder, automobile, or make of automobile.

203. RECORDING OF PENALTIES ON LICENCES

Each penalty imposed shall be recorded on the competition licence. Should the Stewards consider that the number of infringements is significant, they shall forward the licence to Motorsport Australia for review.

204. REMISSION OF SENTENCE

At its sole discretion, the Board of Motorsport Australia shall have the right to waive a disqualification or the unexpired period of a sentence of suspension and to remit part or all of a fine, provided that all avenues of appeal have been exhausted. Nothing in this NCR shall limit Motorsport Australia's right of appeal.

PART XII – PROTESTS**205. RIGHT TO PROTEST**

- (i) The right to protest lies only with a competitor in that event;
- (ii) The lodging of a protest shall not interfere with the application of the NCR; and
- (iii) A separate protest must be lodged against each alleged infringement.

206. LODGING A PROTEST

- (i) A protest shall be lodged in writing, signed by the protester and accompanied by the fee including any necessary bonds as set by Motorsport Australia;
- (ii) A protest against the qualification or eligibility of an automobile shall specify the details of such alleged infringement;
- (iii) A protest may be withdrawn before it is heard in which case the protest fee shall be returned together with any bond, provided dismantling has not commenced;
- (iv) If the protest is upheld the protest fee and bond shall be returned; and
- (v) If the protest is dismissed the fee shall be forfeited unless, in exceptional circumstances, the Stewards determine otherwise.

207. TO WHOM ADDRESSED

A protest shall be addressed to the Stewards. In their absence it may be delivered to the Clerk of the Course for transmission to the Stewards.

208. TIME LIMIT FOR PROTESTS

- (i) A protest against the entry of a competitor or driver, or against the configuration of the course or handicap or make up of a heat, must be lodged at least two hours prior to the start of that competition.
- (ii) A protest against a decision of a scrutineer shall be lodged as soon as practicable after the decision has been announced.
- (iii) A protest against any error or irregularity occurring during a competition or referring to the eligibility of an automobile shall, except in circumstances which the Stewards consider as unreasonable, be made within thirty minutes of the completion of the competition activities.
- (iv) A protest against the compilation, assessment or publication of results of a competition shall be lodged within three days of their publication or within such further time as the Stewards may consider justifiable, unless the Supplementary Regulations specify otherwise.

- (v) A protest relating to the due and proper compilation, assessment or publication of the results of a series of competitions shall be lodged within three days of the first publication of the progressive pointscore for the relevant round of the series or of the final pointscore for the series. Motorsport Australia may, at its sole discretion, amend the results of a series up to the time prescribed for the official presentation of awards for that series or the official publication by Motorsport Australia of the results for that series.

209. DELETED 2007

210. EXAMINATION OF AN AUTOMOBILE OR EQUIPMENT FOLLOWING PROTEST

- (i) An automobile, part thereof or equipment including data ("material") which is the subject of a protest shall be immediately impounded and/or recorded in detail by the Organiser. Upon the commencement of a hearing, the material shall remain under the responsibility of the court until the matter is determined.
- (ii) The examination and report to the Stewards shall be conducted by the appropriate official.
- (iii) The protestor has no right of attendance at the examination.
- (iv) Any disassembly, re-assembly and examination costs as determined by the Stewards shall be payable by the unsuccessful party.

211. DELETED 2007

212. STEWARDS' HEARINGS

- (i) A Stewards' hearing shall take place as soon as practicable.
- (ii) The Clerk of the Course shall notify each party to the issue of the hearing.
- (iii) Each party may attend and be accompanied by witnesses.
- (iv) The Stewards must ensure that notification has been personally received by each party.
- (v) A party to the issue shall present their case in person and shall not be represented by a third party, unless in exceptional circumstances the Stewards determine otherwise.
- (vi) In the absence of any concerned party or of their witnesses, judgment may be made by default.
- (vii) If adjournment is necessary or if judgment cannot be given immediately after the hearing, the parties must be advised of the place and time at which the matter will be determined.

213. DISTRIBUTION OF PRIZES

Prizes and awards may be distributed on the basis of provisional results. Any prize or award not confirmed by the final results must be returned to the Organiser.

214. RE-RUN

Neither the Stewards, a tribunal, a court nor Motorsport Australia shall have power to order or permit a completed competition to be re-run in its entirety.

215. JUDGMENT

Each party concerned in any protest shall be bound by the decision given, subject only to appeal as provided in the NCR.

216. FRIVOLOUS OR VEXATIOUS PROTESTS

If the Stewards determine that a protest is frivolous or vexatious or that the protestor has acted in bad faith they may impose a penalty.

217. DELETED 2007

PART XIII – APPEALS

218. RIGHT OF APPEAL

- (i) The right of appeal against a decision of the Stewards or other Court of First Instance shall lie with a Competitor, Organiser, Driver or other Licence holder provided that:
 - (a) they are the subject of that decision; or
 - (b) they are individually affected by that decision; and
 - (c) they give notice of intention to appeal and lodge an Appeal in accordance with the NCR.

For an appellant to be individually affected by a decision, it must affect them by reason of certain attributes peculiar to them, or by reason of a factual situation which differentiates them from all other persons and distinguishes them individually in the same way as if they are the subject of the decision.

- (ii) An appeal against a decision of the Stewards or of a Disciplinary or Investigatory Tribunal shall be made to Motorsport Australia and shall be heard and determined by an Appeal Tribunal appointed by Motorsport Australia.
- (iii) A party aggrieved by a decision of an Appeal Tribunal shall have the right to seek leave to appeal to AMSAC, in accordance with the NCR.
- (iv) A person or body aggrieved by a decision of the Board of Motorsport Australia shall have the right of appeal to AMSAC only on the alleged grounds that such action is ultra vires or unconstitutional, in accordance with the NCR.
- (v) A party to an appeal heard by AMSAC may have a right of appeal against that decision to the International Court of Appeal in accordance with the ISC.

219. MOTORSPORT AUSTRALIA RIGHT OF APPEAL

- (i) Where a decision is made by the Stewards or by another Court of First Instance and the Board of Motorsport Australia considers that a different decision should have been made, and is satisfied that an Appeal should be brought in the interest of motorsport, Motorsport Australia may bring an appeal to an Appeal Tribunal. In such a case, Motorsport Australia shall cause notice of appeal to be served on the respondent within 30 days after the handing down of the decision, setting out the grounds of the appeal.
- (ii) Where:
 - (a) a party is penalised by an Appeal Tribunal; or
 - (b) a decision or an interpretation of the NCR is given by an Appeal Tribunal and the Board of Motorsport Australia considers that a different decision or interpretation should have been given, and is satisfied that an appeal should be brought in the interest of motorsport,

Motorsport Australia shall have the right to seek leave to appeal to AMSAC.

- (iii) Upon an Appeal under this NCR the Tribunal or AMSAC respectively shall make whatever order it considers fit.
- (iv) No further appeal shall be brought by Motorsport Australia against any order made by AMSAC under (iii) hereof.

220. JURISDICTION OF AMSAC AS TO APPEALS

AMSAC shall, subject to the laws of Australia, constitute the final motorsport court of appeal empowered to settle finally any dispute arising out of or in connection with the control by Motorsport Australia of automobile competitions within its territory save as provided for in the ISC.

221. MEMBERS OF JUDICIAL BODIES TO BE IMPARTIAL

No person may sit on a hearing if they have been involved in any way with the competition or with the matter under consideration.

222. TIME LIMITS AND PROCEDURES FOR APPEAL

- (i) Where it is wished to appeal against a decision of the Stewards, or a decision of a State Council Disciplinary or Investigatory Tribunal, each established as a Court of First Instance:
 - (a) notice of intention to appeal must be lodged with the party giving the decision:
 - (A) within one hour of the handing down of the decision;
 - (B) in writing, on the "Notice of Intention to Appeal" form and signed by the appellant (refer to the Motorsport Australia website, www.motorsport.org.au); and
 - (C) accompanied by the appropriate appeal fee.
 - (b) the Appeal must then be lodged:
 - (A) within two days of the day on which the decision being appealed against was given;
 - (B) in writing on the "Appeal" form and signed by the appellant (refer to the Motorsport Australia website, www.motorsport.org.au); and
 - (C) at the Motorsport Australia office in the State in which the decision appealed against was handed down or in the State of residence of the appellant.
- (ii) Where a Notice of Intention to Appeal and the relevant fee have been lodged, but no Appeal has been received by Motorsport Australia within the time limit, the Notice of Intention to Appeal shall be deemed to have been abandoned and the relevant fee may be returned.
- (iii) In extraordinary circumstances, the CEO of Motorsport Australia may accept a notice of Intention to Appeal or an Appeal lodged out of time.
- (iv) Where it is wished to appeal to AMSAC Notice of Intention to do so is not required, however an Application for Leave to Appeal must be lodged in accordance with Appendix G (see "Judicial" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).
- (v) Where an Appeal fails or is withdrawn after being lodged the appeal fee shall not be returned. Additionally the appellant shall be liable for any costs reasonably incurred by Motorsport Australia in consequence of the lodgement of the appeal.
- (vi) Where an Appeal is upheld the fee may be returned in whole or in part at the discretion of the court.
- (vii) If the court or Motorsport Australia determines that an appeal is frivolous or vexatious or that the appellant has acted in bad faith it may impose a penalty.

223. STATUS OF APPELLANT

A sentence of suspension or of exclusion shall be set aside until the matter is determined; however any other decision or penalty shall remain in force until the matter is resolved.

224. DELETED 2007

225. GROUNDS FOR APPEAL

- (i) An appeal may proceed only on one or more of the following grounds:
 - (a) that the decision was against the weight of evidence;
 - (b) that the decision was contrary to the NCR;
 - (c) that there was a denial of natural justice;
 - (d) that the decision was ultra vires;
 - (e) that the penalty applied (other than one fixed by regulation) was inadequate; and
 - (f) that the penalty applied (other than one fixed by regulation) was excessive.
- (ii) Such grounds must be specified in the Appeal document.

226. APPEAL TO THE FIA

Refer to the ISC.

227. NOTICE OF APPEAL HEARING

- (i) Before an appeal hearing shall commence each party shall be given at least seven days' notice in writing of the date, time and place of the appeal hearing, save that a hearing may proceed on shorter notice if all parties agree.
- (ii) In the case of an appeal to AMSAC, refer to Appendix G (see "Judicial" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).

228. HEARING OF AN APPEAL BY MOTORSPORT AUSTRALIA OR TRIBUNALS

- (i) A hearing shall take place as soon as practicable.
- (ii) Motorsport Australia shall notify each party of the hearing.
- (iii) Each party may attend and be accompanied by witnesses.
- (iv) At an appeal hearing each party shall state their case in person, save that:
 - (a) representation by an advocate may be granted by the chairman of the appeal panel;
 - (b) application for such leave must be made in writing at least two days prior to the hearing through the office of Motorsport Australia which set up the appeal hearing; and
 - (c) the decision by the chairman of the appeal panel on such representation shall be final and not subject to appeal.

229. DELETED 2007

230. HEARING OF AN APPEAL BY AMSAC

Refer Appendix G (see "Judicial" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).

231. JUDGMENT ON APPEAL

- (i) When handing down its findings a court shall include its reasons therefore.
- (ii) A court of appeal and any tribunal may decide that a penalty appealed against (other than one fixed by any regulation) be set aside, mitigated, increased or a fresh penalty imposed. It may not vary a decision made by the Board of Motorsport Australia save that AMSAC may do so only as provided in Appendix G (see "Judicial" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).

232. ORDER AS TO APPEAL FEE

When giving judgment on an appeal any court shall, unless it deems special circumstances to exist which justify the return of such fees or part of them, order the forfeiture of the appeal fee by an unsuccessful appellant; save that in an Appeal to AMSAC, the provisions of Appendix G (see "Judicial" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au) shall apply.

233. FAILURE TO PROCEED WITH AN APPEAL

Failure on the part of an appellant to proceed with an appeal, whether by a lapse of time or otherwise, shall restore the decision against which the appeal was lodged.

234. PUBLICATION OF JUDGMENT

- (i) Motorsport Australia shall have the right to publish or have published the outcome of an appeal and to state the names of all the parties involved.
- (ii) Without prejudice to any right of appeal any person referred to in such notices shall have no right of action against Motorsport Australia or against any person publishing the said notice.

PART XIV – ADMINISTRATION OF RULES

235. INTERPRETATION OF RULES

In order to:

- (i) promote the sport of automobilism;
- (ii) encourage and control automobile competitions;
- (iii) achieve prompt final results in automobile competitions; and
- (iv) ensure the receipt by members of the benefits and rights available to them each member and associated and regulated body expressly agrees that any determination by the Board of Motorsport Australia as to the interpretation and application of the NCR is binding and not open to challenge before a court in law or in equity, and covenants that they will not initiate or maintain or commence legal proceedings, claims or actions of any kind against Motorsport Australia or any party acting on behalf of Motorsport Australia to reverse or modify such determinations or to recover damages or any other remedy for damage allegedly incurred as a result of such determination unless the determination was made in bad faith and for no purpose other than to cause harm or economic loss to the member or body.
- (v) Should legal proceedings be commenced or maintained in contravention of the above, Motorsport Australia shall be indemnified by the member or body who commenced such proceedings for all costs incurred by Motorsport Australia in defending the proceedings.

236. EXERCISE OF THE POWERS OF MOTORSPORT AUSTRALIA

- (i) The executive and judicial powers and functions conferred on Motorsport Australia by the FIA shall be exercised by the Board of Motorsport Australia in accordance with the Constitution and the NCR.
- (ii) The Board of Motorsport Australia may formulate, issue, adopt and modify such NCR for the proper management of Motorsport Australia, the advancement of the purposes of Motorsport Australia and the control of motorsport in Australia as it may deem necessary or desirable.
- (iii) The NCR shall be consistent with the Constitution.
- (iv) Motorsport Australia may delegate judicial powers and functions.

237. AWARD OF COSTS

When giving judgment on any charge or appeal any tribunal or AMSAC may make such order as to costs as it may deem fit against a person or body found guilty or unsuccessful in an appeal. Any delay in the payment of such costs shall entail suspension for the period during which they remain unpaid. Provided always that costs shall not be awarded against Motorsport Australia unless Motorsport Australia itself is an unsuccessful appellant, even though Motorsport Australia may be a party to the hearing in some other capacity.

238. MODIFICATION OF RULES

Motorsport Australia reserves the right to modify the NCR. Each such modification shall be published on the Motorsport Australia website, www.motorsport.org.au

239. NOTICES

- (i) Any communications required under the NCR to be made to Motorsport Australia shall be addressed to:
The Chief Executive Officer Motorsport Australia
PO Box 172, CANTERBURY LPO VIC 3126;
or to such other address as may be notified.
- (ii) Any communications required under the NCR to be sent to the Australian Motor Sport Appeal Court shall be addressed to:
The Secretary Australian Motor Sport Appeal Court
PO Box 172, CANTERBURY LPO VIC 3126;
or to such other address as may be notified.
- (iii) Any communications required under the NCR to be sent to a State Council shall be sent to the address of the State Council concerned (see "Administration" in the Motorsport Australia Manual, refer to the Motorsport Australia website www.motorsport.org.au).

- (iv) Any communications required to be sent to a licence holder by Motorsport Australia under the NCR shall be sent to the address notified to Motorsport Australia.
- (v) Any communications required to be sent to a Promoter or Organiser under the NCR shall be sent to the address shown on the relevant application for an organising permit.
- (vi) Any communications required to be sent to an appellant shall be sent to the address shown upon the Notice of Appeal.
- (vii) Any communications so sent shall be deemed to have reached the addressee by normal delivery.

240. EXTENSION OF TIME

Where the time for doing any act or taking any proceedings expires when the appropriate Motorsport Australia Office is closed that time shall be extended to the next day that office is open.

241. DATE OF OPERATION

These NCR shall come into force and be operative from the first day of January 2020 and shall be deemed to supersede all previous NCR.



2019 Motorsport Australia NSW Motor Race Championships - Wakefield
 Photo: Ricciardo Benvunuti



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KEEP UP TO DATE ONLINE

To ensure you are equipped with the latest Motorsport Australia regulations, visit motorsport.org.au/regulations/manual.

The online Motorsport Australia Manual is the most up to date source of Motorsport Australia motorsport rules and requirements, with amendments to the Manual clearly marked in the online PDF's.

See also “Definitions – General”, NCR, Part II

BALLAST

Completes the weight of the automobile by one or several ballasts on condition that each is a strong and unitary block, fixed by means of tools with the possibility to affix seals.

BALLAST RETENTION

Retention of ballast shall be by a minimum of class 8.8 M6 bolts & lock nuts up to a single ballast weight of 18kg and a minimum of M8 bolts and lock nuts for a single ballast weight of 18kg to 65Kg. A reinforcing steel plate of at least 75mm x 50mm x 3mm under each bolt and nut shall be present. When a single bolt is used, it shall be centrally located in the ballast. Where the ballast top face surface area exceeds the surface area size of a reinforcement plate by 2 1/2 times, then a minimum of two bolts shall be used and located evenly across the ballast.

BODY SHELL

The main coachwork structure of an automobile which, in the case of an automobile not having a separate chassis, constitutes the fundamental structure of the automobile. Components such as doors, bonnet, bootlid and mudguards which are readily demountable are not deemed to be part of the body shell.

BODYWORK/COACHWORK

- (a) Externally: all the entirely suspended part of the automobile liked by the airstream;
- (b) Internally: cockpit and boot.

Bodywork is differentiated as follows:

- (a) Completely closed bodywork;
- (b) Completely open bodywork;
- (c) Convertible bodywork with the hood in either supple (drop-head) or rigid (hardtop) material.

CHASSIS

The structure of any automobile so constructed that the coachwork is a separate entity and not a primary load carrying element of the automobile. It foresees that the coachwork may contribute to the overall strength of the automobile, but the word “chassis” is applicable only to those automobiles in which removal of the coachwork does not affect the entity of the mechanical components of engine, transmission, suspension and unsprung part as an assembly.

COCKPIT

Structural inner volume which accommodates the driver and the passengers.

COMPOSITE MATERIALS

- (a) Material formed from several distinct components, the association of which provides the whole with properties which none of the components taken separately possesses. More specifically, these are materials where a matrix material is reinforced by either a continuous or discontinuous phase.
- (b) The matrix can be metallic, ceramic and polymeric or glass based.
- (c) The reinforcement can be present as long fibres (continuous reinforcement) or short fibres, whiskers and particles (discontinuous reinforcement).

CROSS MEMBER

A transverse member which adds support to a structure.

CYLINDER BLOCK

The crankcase and the cylinders.

CYLINDER CAPACITY

Volume V generated in cylinder (or cylinders) by the upward or downward movement of the piston(s).

$$V = 0.7854 \times b^2 \times s \times n$$

where: b = bore
s = stroke
n = number of cylinders

ELASTOMER

An elastic solid composed primarily of hydrocarbon material with widely-spaced cross-linking bonds. Such material shall have a maximum Shore (A) Hardness of 100, and a maximum tensile strength of 60MPa.

ELASTOMERIC BUSHING

A flexible coupling between two rigid structures that provides limited radial and axial freedom of movement. Bushings with less than 4.0mm of elastomer between the rigid structures shall not be regarded as elastomeric.

ELECTRIC VEHICLE

An EV uses one or more electric motors or traction motors for propulsion. An EV may also utilise forms of regenerative charging to recharge and propel or retard (brake) the vehicle. They are defined for motorsport as one of the following:

Series Production EV: An EV produced by a manufacturer, approved for and able to be registered for general road use.

Competition EV: An EV produced solely for the purpose of competition in motorsport.

An EV may be further defined as:

Hybrid EV: the electric motor provides tractive force to move the vehicle in conjunction with or independent of an internal combustion (IC) engine. The IC engine, in conjunction with a generator, provides the charging for the battery pack.

Plug-In Hybrid EV: the battery pack is rechargeable by being plugged into an external power source. The tractive force to move the vehicle may then be provided by the electric motor or supplemented by an IC engine, which may also provide charging through a generator.

Battery EV: the battery pack is rechargeable by being plugged into an external power source. The tractive force to move the vehicle is provided only by an electric motor.

EXHAUST MANIFOLD

Part collecting together at any time the gases from at least two cylinders from the cylinder head and extending to the first gasket separating it from the rest of the exhaust system.

For cars with a turbocharger, the exhaust begins after the turbocharger.

FAMILY OF AN AUTOMOBILE

Different series models belonging to one and the same production series of the same manufacturer. Not fewer than the number of automobiles specified in the relevant Technical Regulations for the category of automobile with the same external general lines of the bodywork, material of the bodywork and wheelbase must have been produced in 12 consecutive months. All models must be available through the normal commercial channels of the manufacturer.

Variations in the following details are acceptable:

- (a) Shape and material of front and rear bumper bars
- (b) Removable aerodynamic devices (e.g. spoilers, wings, sill mouldings)
- (c) Control and comfort equipment (e.g. sun roof, auxiliary lamps, door handles, exterior mirrors)
- (d) Decorative strips and mouldings
- (e) Left and right hand drive versions
- (f) Versions which differ in the number of doors provided that these differ only with regard to the doors, door openings and pillars
- (g) Versions with different engine and drivetrain configuration.

FRICTION SURFACE OF THE BRAKES

Surface swept by the linings on the drum, or the pads on both sides of the disc, when the wheel achieves a complete revolution.

FUEL TANK

Any container holding fuel likely to flow by any means whatsoever towards the main tank or the engine.

HOT LIQUID

Any liquid being of a temperature likely to cause at least first degree burns on contact with the skin.

ID

Inside diameter.

IDENTICAL AUTOMOBILES

Automobiles belonging to the same production series and which have the same bodywork (outside and inside), same mechanical components and same chassis (even though this chassis may be an integral part of the bodywork in case of a monocoque construction).

INTAKE MANIFOLD

- (a) Carburettor System: the components collecting the air-fuel mixture from the carburettor/s, and extending to the inlet ports.
- (b) Injection System: the components collecting the air from the air intake control device and extending to the inlet ports.
- (c) Diesel Engine: the components collecting the air at the air filter and extending to inlet ports.

LUGGAGE COMPARTMENT

Any volume distinct from the cockpit and the engine compartment inside the vehicle. This volume is limited in length by the fixed structures provided for by the manufacturer and/or by the rear of the seats.

This volume is limited in height by the fixed structures and/or by the detachable partitions provided for by the manufacturer, or in the absence of these, by the horizontal plane passing through the lowest point of the windscreen.

MAIN STRUCTURE

The fully sprung structure of the vehicle to which the suspension and/or spring loads are transmitted, extending longitudinally from the foremost front suspension mount to the rearmost mount of the rear suspension.

MANUFACTURING STANDARDS

Unless specifically authorised in the relevant Technical Regulations for the Group or Category, it is not permitted to modify any component, even though the end result may fall within a permitted range.

MAXIMUM VALUE

Where a quantity is specified as having a maximum value, that value shall be absolute and no tolerance shall apply.

MEASURING TOLERANCES

Where a tolerance is expressed in the recognition documents, it shall apply, otherwise the following is applicable:

Bore and stroke	± 0.1mm
All machining (except bore and stroke) including fan, crankshaft bearings, connecting rod bearings, valves, ports, carburettor, venturi, manifolds and clutch:	± 0.2%
Distance from gudgeon pin centre line to highest point of piston crown:	± 0.5%
Unfinished castings:	+4% - 2%
Cam lift:	+ 1%
Weight of flywheel, clutch, crankshaft, connecting rods and pistons:	+7% - 0.3%
Width of car at front and rear axles:	+1% - 0.3%
Wheelbase:	± 1%
Track:	± 25mm

MECHANICAL COMPONENT

Any component of an automobile whether moving or not, which is necessary for the propulsion, suspension, steering and braking as well as any accessory which may be used in its operation.

MECHANICALLY IDENTICAL COMPONENT

A component which performs exclusively the original function/s in the same manner as foreseen by the manufacturer and which permits the attachment of any secondary components in the original manner and without modification of that component.

MINIMUM VALUE

Where a quantity is specified as having a minimum value, that value shall be absolute and no tolerance shall apply.

MINIMUM WEIGHT

The weight of the empty automobile (without persons, luggage, tools or jack aboard). Any reservoir containing a liquid (e.g. lubrication, cooling, brake fluid and heating if necessary) shall be filled to the level laid down by the manufacturer, with the exception of the windscreen or headlight washer, brake cooling system, fuel and water injection/intercooler spray system, which shall be empty.

MODEL OF AUTOMOBILE

All the identical automobiles belonging to a family (see Family of an Automobile) and a production series distinguishable by an identical conception and identical external general lines of the coachwork, and by an identical mechanical conception of the engine and the transmission to the wheels.

MONOCOQUE

A form of motor vehicle body construction in which all or most of the stresses are carried by the skin.

OD

Outside diameter.

OPEN AUTOMOBILE

An automobile without a supporting structure between the tops of the windscreen pillars and those of the rear window (if fitted).

PERIMETER OF AN AUTOMOBILE

The location delineating the horizontal extremities of an automobile.

PRODUCTION CAR

An automobile of which the production of a certain number of identical examples within a specified period of time has been verified and which are destined for normal sale to the public. In competition, such an automobile shall retain the basic bodyshell, suspension and driveline components.

RACING WEIGHT

The weight of the automobile during or immediately after a competition including the driver wearing all normal racing apparel including helmet. No materials, liquid or otherwise, may be added prior to weighing.

ROCKER PANELS

The external body panel extending horizontally from front to rear mudguard panels, and from sill to the lower extremity of the coachwork, when the automobile is viewed in side elevation.

ROTARY (WANKEL-TYPE) ENGINE

Spark ignition engine based on the Wankel principle.

RUNNING GEAR

The running gear includes all parts totally or partially unsuspended.

SEAL

Element used for identifying components of a vehicle for either of the following purposes:

- (a) control of the use or replacement of a component;
- (b) follow up of the number of components used or registered as required by the applicable regulations;
- (c) registration of a component seized for carrying out immediate or deferred technical checks;
- (d) prevent the dismantling and/or the modification of a component or part of an assembly; or
- (e) any other need for the application of technical and/or sporting regulations.

SEAT

The two surfaces making up the seat cushion and seatback or backrest.

SILL

The component of the body shell, generally in a horizontal plane, which constitutes the lower extent of a door opening.

SPACE FRAME CHASSIS

An automobile chassis so constituted that all loads are borne by a matrix of structural sections of metal.

SPLITTER

An aerodynamic device generally mounted horizontally to the front lower bodywork of an automobile and which is contiguous therewith.

SPOILER

An aerodynamic device attached to an automobile which is contiguous with the bodywork and which is licked on only one surface by the airflow.

SPORTS CAR

An automobile designed primarily for road use with at least two (2) seats equally disposed about the centreline of the automobile capable of seating two adults.

STATIC GASKET

The only function of a gasket is to ensure the sealing of at least two parts, fixed in relation to each other. The distance between the faces of the parts separated by the gasket must be less than or equal to 5 mm.

STRESSED SKIN SPACE FRAME CHASSIS

A space frame chassis to which stress bearing panels are attached.

SUB FRAME

A supporting frame that uses a discrete, separate structure within a chassis or monocoque to carry certain components, such as the engine, drivetrain, or suspension. A Sub-frame may be bolted and/or welded to the other structure/s of an automobile. When bolted, it may be equipped with elastomer bushings.

SUPERCHARGING (inc. TURBOCHARGING)

A mechanical device capable of producing positive (above atmospheric) pressure in the induction system, i.e. any device which effects a measurable increase in the Brake Mean Effective Pressure (BMEP).

An air duct which delivers air to the engine intake is not considered to be a supercharger.

TRACK

The distance between the centres of the contact patches of the tyres on the same axle as presented for competition.

WHEEL ANGLES - LIVE REAR AXLES

Unless established otherwise by the manufacturer or included in the relevant regulations, all production-based automobiles utilising a live rear axle are deemed to be configured with parallel wheel planes.

WHEEL

Wheel: flange and rim.

Complete Wheel: flange, rim and tyre. For measurement the tyre shall be inflated to the tyre manufacturer's recommended pressure.

WING

An aerodynamic device attached to an automobile which is licked on both upper and lower surfaces by the airflow.

CLASSIFICATION OF AUTOMOBILES

1. RECORDS

For the purpose of attempting Motorsport Australia speed and/or endurance records, automobiles will be classified according to their calculated effective engine capacity, using the relevant factors as noted below. Automobiles shall then be placed in one of the following classes:

Vehicles with positive displacement reciprocating engines:

Below 250cc	Class 1
251-350cc	Class 2
351-500cc	Class 3
501-750cc	Class 4
751-1100cc	Class 5
1101-1500cc	Class 6
1501-2000cc	Class 7
2001-3000cc	Class 8
3001-5000cc	Class 9
5001-8000cc	Class 10
Over 8000cc	Class 11

(See "Supercharging", below)

2. GENERAL

Applicable to all competition (excluding records) unless specifically noted otherwise.

2.1 ROTARY COMBUSTION (WANKEL-TYPE) ENGINES

The nominal engine capacity will be calculated by the following formula: 1.8 times the volume determined by subtracting the minimum capacity of the working chamber/s from its/their maximum capacity unless modified by specific category regulations.

2.2 SUPERCHARGING

The nominal cylinder capacity for a supercharged engine shall be multiplied by a factor of:

(a) for spark ignition engines = 1.7

(b) for Diesel engines = 1.5

and the automobile will be classified in all respects corresponding to the effective capacity thus obtained, unless modified by specific category regulations.

2.3 AUTOMOBILES WITH ELECTRIC, TURBINE AND STEAM OR HYBRID ENGINES

Class allocation for competition purposes will be made on application to Motorsport Australia.

3. COMPETITIONS

- (a) For international competitions, the class structure shall be in accordance with the ISC.
- (b) For other competitions, automobiles shall be classified as below.
- (c) Event organisers are permitted to amalgamate any adjoining classes, but shall use only the class limits stated for the relevant category or group.
- (d) Competitions shall be conducted only in accordance with the following categories, unless varied by Motorsport Australia.

3.1 1ST CATEGORY: RACING CARS

- (a) Formula Ford, Formula Ford 1600
- (b) Formula Vee
- (c) Formula 3
- (d) Formula Libre for Speed Events
 - (i) Up to 1300cc
 - (ii) 1301-1600cc
 - (iii) 1601-3000cc
 - (iv) 3001-5000cc
 - (v) Above 5000cc
- (e) Free Formula for Race Events
 - (i) Up to 1300cc
 - (ii) 1301-1600cc
 - (iii) 1601-3000cc
 - (iv) 3001-5000cc
- (f) Formula 4
- (g) S5000

3.2 2ND CATEGORY

- (a) Sports Cars
 - (i) Up to 1300cc
 - (ii) 1301-1600cc
 - (iii) 1601-3000cc
 - (iv) 3001-6000cc
 - (v) Above 6000cc
- (b) Prodsports Cars:
 - (i) Up to 1300cc
 - (ii) Up to 1300cc
 - (iii) 1301-1600cc
 - (iv) 1601-3000cc
 - (v) 3001-6000cc
 - (vi) Above 6000cc
- (c) 2GT FIA Homologated Cars:
- (d) 2l Carrera Cup Cars:

3.3 SPORTS SEDANS

- (a) Up to 1600cc
- (b) 1601-2000cc
- (c) 2001-3000cc
- (d) 3001-4000cc
- (e) 4001-6000cc

3.4 3RD CATEGORY: TOURING CARS

- (a) Up to 1300cc
- (b) 1301-1600cc
- (c) 1601-2000cc
- (d) 2001-3000cc
- (e) Over 3000cc
- (f) Otherwise as specified in Group/Category regulations i.e. Rally/Road.

3.5 IMPROVED PRODUCTION CARS

- (a) 0-1600cc
- (b) 1601-2000cc
- (c) 2001-3000cc
- (d) 3001-6000cc
- (e) Over 6000cc

3.6 CIRCUIT EXCEL

As specified in Group regulations.

3.7 HQ HOLDENS

As specified in Group regulations.

3.8 SALOON CARS

As specified in Group regulations.

3.9 TCR

As specified in Category regulations.

3.10 4TH CATEGORY: OFF ROAD AUTOMOBILES

As specified in Group regulations.

3.11 5TH CATEGORY: HISTORIC CARS

As specified in Group regulations.

3.12 6TH CATEGORY: OTHER AUTOMOBILES

As specified in Group regulations.

Includes:

- FIA CN
- LMP3

3.12 7TH CATEGORY: OTHER AUTOMOBILES

- (a) Drift
- (b) Speed Event Other
- (c) Road Registered (Standard)
- (d) Road Registered (Modified)

SCHEDULE A

Each automobile (except a superkart) shall, of necessity, in any competition:

- (a) comply with the definition of an automobile;
- (b) be fitted with protection between engine and driver's compartment to prevent the passage of flame;
- (c) be so constructed to minimise the entry of foreign matter into the driving compartment from the road or road wheels;
- (d) have any propeller shaft and universal joints, if passing through the cockpit, fitted in a fixed casing;
- (e) have any driving chain effectively guarded;
- (f) have each fuel tank vented externally to the bodywork;
- (g) if manufactured prior to 1 January 1978 (or otherwise not complying with ADR25A) and not registered for use on public roads, have any steering column locking device removed or disabled;
- (h) be fitted with a safety cage as required by Schedule J;
- (i) use only fuel compliant with Schedule G;
- (j) have any window or windscreen fitted made from a material which is clear or, if tinted, compliant with AS 2080;
- (k) have a safety harness as required by Schedule I;
- (l) have any container within the cockpit which can hold more than 500mL of hot liquid (other than a series heater core) enclosed in a sealed compartment;
- (m) where fitted with rigid brake pipes have such pipes made of steel ('Bundy' tubing or equivalent), unless it is an automobile of the 5th Category which is fitted with original components. The installation must be such to protect the pipes against vibration and damage; and
- (n) be fitted with ballast in compliance with the requirements detailed in Definitions-Technical.
- (o) be, for an Electric Vehicle, compliant with the Electric Vehicle Standing Regulations.

KEEP UP TO DATE ONLINE

To ensure you are equipped with the latest Motorsport Australia regulations, visit motorsport.org.au/regulations/manual.

The online Motorsport Australia Manual is the most up to date source of Motorsport Australia motorsport rules and requirements, with amendments to the Manual clearly marked in the online PDF's.

SCHEDULE B

Each automobile (except a superkart) shall, of necessity, in any speed event or race:

- (a) be fitted with two separate fastening systems on any bonnet or other panel where the leading edge can be raised. The fastening systems shall meet the following requirements:
 - (i) to be deemed separate, a fastening system shall continue to function if the second system is removed in its entirety;
 - (ii) they shall be of adequate strength and limited elasticity and range of movement;
 - (iii) they shall simultaneously hold the bonnet or panel closed or as an alternative for speed events only, one fastening system shall hold the bonnet or panel closed and its release shall allow the bonnet or panel to be raised to provide access to a second separate fastening system fitted within the automobile. The second fastening system shall prevent the bonnet or panel from being raised more than 150mm from the fully closed position.

A road registered series production automobile fitted with an unmodified original equipment two stage fastening system shall be exempt from these requirements;
- (b) be fitted with a fire extinguisher compliant with Schedule H;
- (c) be fitted with a device or devices that shall protect any longitudinal propeller shaft from striking the ground in the event of a component failure;
- (d) be fitted with wheels and tyres compliant with Schedule E;
- (e) if fitted with any aerodynamic device, be compliant with Schedule F;
- (f) if fitted with a scatter shield, be compliant with Schedule M;
- (g) be fitted with a return mechanism which in the event of any throttle linkage or throttle system failure will close each throttle;
- (h) be fitted with a dual circuit braking system save for automobiles manufactured prior to 31 December 1973 or of the 5th Category;
- (i) be fitted with an operable reverse gear controlled by the driver whilst seated in the driving position, save for automobiles of the 5th Category and Formula Libre;
- (j) be fitted with sideways or rearward-facing exhaust outlets. If rearwards, the outlet/s shall be between 100mm and 450mm above the ground and shall not protrude more than 150mm beyond the rearmost portion of the automobile. If directed sideways, the outlet/s must be located rearward of the midpoint of the wheelbase. In any case, they shall not project beyond the maximum width of coachwork or terminate more than 50mm within the plan view of the adjacent coachwork;
- (k) save for 1st Category automobiles, be fitted with a bulkhead constructed from a flame - and liquid-proof material. If the material is constructed from polycarbonate it shall be a minimum of 6mm thick. This bulkhead shall effectively seal the cockpit from the fuel tank and re-fueling system.
- (l) be configured such that the sound emitted when measured 30m from the track edge does not exceed 95dB(A) unless event regulations set a lower limit;
- (m) be fitted with a steering wheel not incorporating any wood, unless such is the original component of the automobile;
- (n) if fitted with any crankcase breather discharging to the atmosphere, each breather be vented into a catch tank of minimum capacity of two litres for engines up to 2000cc or three litres for over 2000cc. Regulations for competitions on unsealed surfaces may waive this requirement;
- (o) if fitted with any engine radiator coolant vent discharging to the atmosphere, each coolant vent be vented to a catch tank of a minimum capacity of one litre. Regulations for competitions on unsealed surfaces may waive this requirement;
- (p) if in a Multi-car event each forward facing lamp must not be red in colour, and each external forward-facing glass component, save for the windscreen, must have fitted an adhesive cover of a colour other than red to prevent the spillage of broken glass;
- (q) display a blue triangle of sides 150mm indicating the location of the battery. A battery fitted in the cockpit shall have an additional blue triangle not less than 60mm sides fitted on the cover of the battery or immediately adjacent to the battery if uncovered;
- (r) be fitted with a visible towing point (capable of accepting a 40mm OD cylindrical test object) fitted forward of the front axle and rearward of the rear axle and capable of towing the automobile on a sealed surface with its wheels locked. Where a tow point is obscured, each tow point shall be marked with the word "TOW" of a contrasting colour marking the location of each tow point. A road registered series production automobile fitted with an unmodified original equipment tow point shall be exempt from these requirements, save for the requirement to identify an obscured front and/or rear tow point; and
- (s) for each external door handle that is not easily distinguishable or visible from the surrounding bodywork, there shall be fitted an arrow, a minimum of 50mm long, yellow or red in contrasting colour to the bodywork and the word 'OPEN, LIFT, PUSH or PULL', whichever is appropriate, marking the location and operation of each door handle. The arrow marking the location shall be clearly visible on approach by an event official.

Notwithstanding the above requirements each automobile registered for road use shall not be required to comply with the provisions of sub-sections (c), (e), (f), (h), (i), (k), (n) and (o) of Schedule B when competing in Single- and/or Multi-car Speed Events.

SCHEDULE C

Each automobile in a circuit race (except a superkart) shall, of necessity, in addition to the provisions of Schedules A and B, be fitted with:

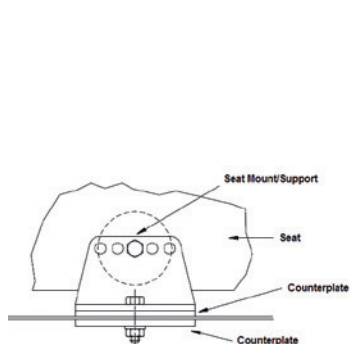
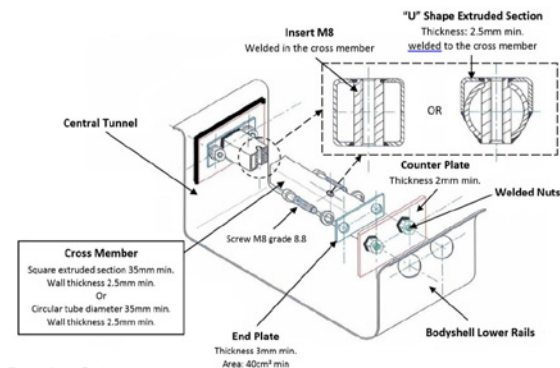
- (a) only with laminated glass in any glass windscreen. Windows, including windscreens, shall not be coloured or tinted unless fitted as standard to a production automobile and compliant with AS2080;
- (b) with wheels and tyres in compliance with Schedule E. No tyre shall have been retreaded, recapped, repaired or re-conditioned in any way;
- (c) with bodywork which generally encloses (when viewed from above and each side) the chassis frame and basic mechanical elements, from the front of the automobile rearwards at least as far as a transverse vertical plane immediately to the rear of the driver's seat;
- (d) with not fewer than two functional rear vision mirrors each of at least 50cm²;
- (e) with a fuel tank as specified in Schedule N;
- (f) only with such replacement seat which in a closed automobile first registered with Motorsport Australia after 1 January 1980, and in which the relevant regulations permit the replacement of the driver's seat and which:
 - (i) incorporates a head restraint; and
 - (ii) does not incorporate adjustment of the rake of the squab.

NOTE: The use of a seat to the FIA 8862-2009 Advanced Racing Seat standard is recommended. Where a seat to this standard is required, the seat shall be used with the seat mount bearing the same FIA homologation number unless an alternative seat mount has been homologated by Motorsport Australia, in which case that seat mount may be used

- (g) with a seat mount/support fixed as per the following:
 - (i) Where an automobile is in compliance with the Australian Design Rules (ADR) for seat mountings, on the anchorage points for fixing seats used on the original automobile; or
 - (ii) On anchorage points for fixing seats in conformity with Drawing C-1; or
 - (iii) Where a seat is affixed to an un-reinforced section of the floor pan, each attachment point shall be reinforced by the use of a plate of not less than 40cm². The minimum thickness of each support and counterplate shall be 3 mm for steel and 5 mm for light alloy material. A seat mount/support must be attached to the shell/chassis using at least 4 mounting points per seat using 8.8 grade bolts with a minimum diameter of 8mm with counterplates, in compliance with Drawing C-2. An anchorage point for fixing a seat may be in compliance with Drawing C-1. A cross member may be welded in place instead of fixing by bolts providing the connection is fully welded around the circumference.
- (h) with a window net as required by Schedule I.

Each automobile in a circuit race (except a superkart) shall, of necessity, also be required to comply with the following:

- (i) On each automobile, other than those of the 5th Category, fitted with an electric fuel pump, the pump power supply must cut off in a maximum of six seconds after the engine stops;
- (i) Each automobile of the 2nd and 3rd Category shall be equipped with a battery isolation (master) switch which isolates the battery and stops the engine, and:
 - (i) it shall be capable of being operated by the driver in their normal seated position;
 - (ii) there shall be a second switch, or a remote means of operating the main switch, from the vicinity of the base of the A pillar on the driver's side or, for an automobile with no A pillar, in a comparable position; and
 - (iii) each external device shall be clearly marked by a symbol showing a red spark in a white-edged blue triangle of minimum edge length 150mm;
- (k) For a non-road-registered series production automobile any cable-operated bonnet or engine cover release mechanism must be disabled and replaced with at least two fastening systems in accordance with Schedule B;
- (l) Each automobile be fitted with a rearward facing red warning lamp (rain light) which must be clearly visible from the rear and mounted not more than 100mm from the centreline of the automobile. Each lamp must:
 - (i) be of at least 15 watts or an LED lamp with FIA and/or Motorsport Australia approval may be used and may strobe;
 - (ii) have a minimum surface area of 20cm² and a maximum surface area of 140cm²; and
 - (iii) be able to be switched on by the driver when normally seated in the automobile.



Drawing C-1

Drawing C-2

SCHEDULE D – APPAREL

The requirement for specified apparel for participants in motorsport is noted in NCR 141. These requirements vary in relation to the overall risk of the activity and, where appropriate, with regard to FIA and/or local civil authority regulations. The tables below shall be used to determine the level and type of apparel required for a given event.

1. APPLICATION

- (a) The letters in the following tables represent the applicable standard as detailed in Article 2. These are referenced by apparel type and status of the competition.
- (b) The standards set out in this Schedule are the minimum requirement for the competition or event listed.
- (c) The status of the application refers to the level of competition entered at a given event.
- (d) A passenger in an automobile shall be required to wear apparel as detailed in the Motorsport Australia Passenger Ride Activity (MSPRA) policy.
- (e) Any international competition listed on the FIA International Calendar will be subject to the standard of apparel detailed in the applicable FIA ISC and these will supersede the requirements set out below should a conflict arise for the event.

1.1 RACE

	Automobile other than a Superkart		Superkart
	Club Multi-Club State National	International	Club Multi-Club State National
Helmet	A	A	C
Frontal Head Restraint	A	A	B
Flame-retardant Overalls / Other Outerwear	B	A	E
Flame-retardant Underwear	B	A	C
Flame-retardant Balaclava	B	A	D
Footwear	A	A	D
Socks	A	A	C
Gloves	A	A	D
Practice / Demonstrations: Apparel requirements are as per the relevant Event.			



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1.2 RALLY/ROAD

1.2 RALLY/ROAD	Gravel / Tarmac Rally			Other	S1 Rallysprint
	Club Multi-Club	State	National	Club Multi-Club State National	Club Multi-Club State National
Helmet	A	A	A	D	B
Frontal Head Restraint	A	A	A	C	B
Flame-retardant Overalls / Other Outerwear	D	C	B	F	D
Flame-retardant Underwear	B	B	B	C	B
Flame-retardant Balaclava	C	B	B	D	C
Footwear	B	B	A	C	B
Socks	B	B	A	C	B
Gloves (not mandatory for co-drivers)	B	B	A	C	B
	Gravel Rally notes: Gravel and Tarmac Rally including Special Stage, Targa Rally, Cross Country Rally and S2 Rallysprint.			Other Rally/Road Event notes: Other rally/road events include Introductory Rally, , Touring Assemblies, Navigation Assemblies, Economy Runs and other Road Rallies complying with civil road rules. For a Road Rally (Touring Road Event) the apparel requirements applicable to the sub-event being conducted shall apply.	
	Practice / Demonstrations: Apparel requirements are as per the relevant Event.				

1.3 OFF ROAD

	Club Multi-Club State National
Helmet	A
Frontal Head Restraint	A
Flame-retardant Overalls / Other Outerwear	C ¹
Flame-retardant Underwear	B ¹
Flame-retardant Balaclava	B ²
Footwear	B
Socks	A
Gloves	A
Off Road notes: ¹ If a SFI 3-2A/1 or a single layer flame retardant overall is worn, level A flame retardant underwear must also be worn. ² Level A required for any Open Face Helmet.	
Practice / Demonstrations: Apparel requirements are as per the relevant Event.	

1.4 SPEED (INCLUDES DRIFTING AND REGULARITY)


















	Club Multi-Club State National	'FHR for SPEED Events A FHR compliant with Standard A and a Helmet compliant with Standard A must be worn when competing in: (a) Each 1st Category (single seater) automobile, except for: (i) a Superkart; or (ii) a 5th Category automobile which is exempt from the use of a FHR. (b) Each automobile the subject of a Motorsport Australia Log Book which applies a RACE, RALLY/ROAD, OFF ROAD or 5th CATEGORY classification that requires the mandatory use of a FHR. The use of a FHR is not required for a Road Registered automobile for a SPEED Event, except where Article (b) above is applicable.
Helmet ¹	A/B	
Frontal Head Restraint ¹	A/B	
Flame-retardant Overalls / Other Outerwear	D	
Flame-retardant Underwear	B	
Flame-retardant Balaclava	C	
Footwear	B	
Socks	B	
Gloves	B	
Practice / Demonstrations: Apparel requirements are as per the relevant Event.		A full-face helmet is highly recommended for a driver of an open car.

1.5 AUTOTEST

	Motorkhana	Khanacross
	Club Multi-Club State National	Club Multi-Club State National
Helmet	D/B ¹	B
Frontal Head Restraint	C	C
Flame-retardant Overalls / Other Outerwear	F	F/D ²
Flame-retardant Underwear	C	C
Flame-retardant Balaclava	D	D
Footwear	C	C
Socks	C	C
Gloves (not mandatory for co-drivers)	C	C
Motorkhana notes: ¹ Each driver and (where applicable) passenger of a motorkhana special is required to wear a helmet to Level B standard.		Khanacross notes: ² Drivers and passengers in open automobiles are required to comply with Level D outerwear.
Practice / Demonstrations: Apparel requirements are as per the relevant Event.		

2. APPAREL STANDARDS

2.1 HELMET



Lvl	Standards	Example Label	FHR Compliant
A	Note: For International level Events, only a helmet as specified in FIA Technical List No. 25 (Recognised Standards for Helmets) shall be accepted.		
	FIA 8860-2018 and 8860-2018-ABP		Yes
	FIA 8860-2010		Yes
	FIA 8860-2004 (Not valid level A after 31/12/2020)		Yes
	FIA 8859-2015		Yes
	Snell SA2010 (not valid level A after 31/12/2023) And either FIA 8858-2010 OR FIA 8858-2010 tether anchors (No. etched on the tether anchor)	 	Yes
	Snell SA2010 (Not valid level A after 31/12/2023) and FIA 8858-2002 tether anchors (No. etched on the tether anchor)	 	Yes
	Snell SA2015 and FIA 8858-2010 tether anchors		Yes
B	Snell SAH 2010 (Not valid level A after 31/12/2023) and FIA 8858-2010 tether anchors		Yes
	Helmet of the Level A Standard		
	Snell SA2005 If used with FHR only fitted with compliant FHR tether anchors – FIA 8858-2002 or FIA 8858-2010.		Yes
	BS 6658-85 A/FR If used with FHR only fitted with FHR tether anchors from the manufacturer.		Yes
	SFI 31.1, SFI 31.1A, SFI 31.2A, SFI 24.1 (youth helmet standard) Level A Standard		No
	Snell SA2000		No
	AS/NZS1698 and updates <i>Label may vary depending on approval body</i>		No
	ECE 22 (with 03, 04 or 05 amendments) <i>Label may be on the helmet strap and consist of the E mark with a number which defines the country of origin</i>	  i.e. 4 = Netherlands	No

Lvl	Standards	Example Label	FHR Compliant
C	Full face helmet of the Level A Standard		
	Full face helmet of the Level B Standard		
	Full Face Helmet of Snell M2000, M2005, M2010, M2015 standard (valid for Superkart competition only)		No
D		Not Mandatory	

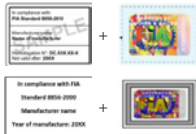
2.2 FRONTAL HEAD RESTRAINT (FHR)

Lvl	Standards	Example Label
A	FIA 8858-2010 <i>Hologram compulsory on 8858-2010 yoke manufactured after 01/01/2013</i> FIA 8858-2002	
B	Not mandatory, Level A standard device highly recommended	
C	Not mandatory, highly recommended where circumstances are appropriate	

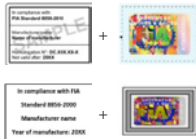
2.3 FLAME-RETARDANT OVERALLS / OTHER OUTERWEAR

A	FIA 8856-2018 <i>Hologram compulsory for all items manufactured.</i> FIA 8856-2000 <i>Manufactured before 01/01/2016 do not require a FIA hologram. (Fig 1)</i> <i>Hologram compulsory for items manufactured after 01/01/2016 (Fig 2)</i>	 (Fig 1) (Fig 2)
B	Level A; FIA 1986	
C	Level A; Level B; SFI3.2A/1 (single layer) or superior SFI standard	
D	Clothing from ankles to neck to wrists. Clothing of flammable synthetic material, such as nylon, is not acceptable. Level A, B or C overalls are recommended.	
E	Minimum of a CIK Level 2 Fabric Overall which must be marked as per the CIK-FIA requirements; or Leather overall , which may be one or two piece, securely fastened at the wrist and ankle and also at the waist of a two-piece suit. In each case the overall must cover the whole body from ankle to neck to wrists.	
F	Minimum of short sleeve shirt / t-shirt and short pants	





2.4 FLAME-RETARDANT UNDERWEAR

A	FIA 8856-2018 <i>Hologram compulsory for all items manufactured.</i> FIA 8856-2000a <i>Hologram compulsory for items manufactured after 01/01/2016</i>	
B	Not mandatory; FIA-standard flame-retardant underwear is recommended	
C	Not mandatory	


2.5 FLAME-RETARDANT BALACLAVA

A	FIA 8856-2018 <i>Hologram compulsory for all items manufactured.</i> FIA 8856-2000 <i>Hologram compulsory for items manufactured after 01/01/2016</i>	
B	Level A balaclava is required, except where the driver/co-driver is wearing a helmet to Level A standard. If a Level A helmet is worn, the wearing of a flame-retardant balaclava is not mandatory, but is recommended	
C	Not mandatory; a motorsport standard balaclava is recommended	
D	Not mandatory	





2.6 FOOTWEAR

Lvl	Standards	Example Label
A	FIA 8856-2018 Hologram compulsory for all items manufactured. FIA 8856-2000 Manufactured before 01/01/2016 do not require a FIA hologram. (Fig 1) Hologram compulsory for items manufactured after 01/01/2016 (Fig 2)	 +   + 
B	Level A or shoes with leather uppers that cover the foot. Shoes which have a leather upper but which include elasticised ankle regions are acceptable (e.g. elastic-sided work boots)	
C	Enclosed shoes	
D	Abrasion-resistant footwear which covers and protects the ankles	

2.7 FLAME-RETARDANT SOCKS

A	FIA 8856-2018 Hologram NOT compulsory for socks. FIA 8856-2000 Hologram NOT compulsory for socks.	 + 
B	Not mandatory; FIA-standard flame-retardant socks are strongly recommended	
C	Not mandatory	

2.8 GLOVES

A	FIA 8856-2018 Hologram compulsory for all items manufactured. FIA 8856-2000 Hologram compulsory for items manufactured after 01/01/2016	 +   + 
B	Not mandatory; FIA-standard flame-retardant gloves are strongly recommended	
C	Not mandatory	
D	Abrasion-resistant gloves that cover and protect the hands and wrists (including the fingers)	

3. FURTHER REQUIREMENTS

These requirements are in addition to the other Articles of Schedule D.

3.1 GENERAL

- Each driver/competing crew member shall continue to comply with the requirement of NCR 141 until such time as they exit their automobile, or for rally/road events until the completion of a competitive section/stage – refer also to Table 1.2.
- Apparel shall be worn as the manufacturer intended.
- Individual category or event regulations may require higher standards of apparel than those detailed in Article 1 and 2. In such cases the higher standard shall apply.

3.2 BADGES AND EMBROIDERY OR PRINTING/TRANSFERS

The attachment of badges, use of embroidery or any printing/transfers on competitor's overalls can have a detrimental effect on the protection afforded by the garment.

- Badges:** For apparel complying to the FIA 8856-2000 or 8856-2018 standard, it is mandatory that all badges attached to the garment have a flame-resistant backing and be attached only with flame-resistant thread to the outer layer of the garment only. For all garments the number and size of badges should be kept to the minimum required to meet commercial and regulatory obligations.
- Embroidery:** For apparel complying to the FIA 8856-2000 or 8856-2018 standard, it is mandatory that all embroidery be done using flame-resistant thread. Embroidery is only permitted to the outer layer of the garment. This is strongly recommended for all apparel.
- Printing/Transfers:** For apparel complying to the FIA 8856-2000 8856-2018 standard any printing or transfer is permitted to be done only by the manufacturer. A competitor may be asked to demonstrate proof that any printing or transfer has been done only by the manufacturer.

3.3 HELMET REQUIREMENTS

(a) Style:

- In addition to the helmets approved for use above, each helmet shall be one of the following design types; full face or open face. An open-faced helmet is defined as a helmet that, as a minimum, covers the hairline of the average person and includes covering the ears.
- Specifically excluded are helmets that do not cover, when viewed in side elevation, the area from the top of the ear to the bottom of the jaw, commonly referred to as a 'skull cap' style.

(b) Modifications:

- A helmet shall not be modified from its specification as manufactured except in compliance with instructions approved by the manufacturer and one of the standards organisations listed in this schedule, which certified the helmet concerned. Any item added or attached to a helmet (including any camera or camera mount) by any means shall be considered as a modification. Any modifications or additions undertaken may invalidate the helmet certification.

- (ii) Approved modifications may include: Painting or wrapping (vinyl sticker or similar) in compliance with the manufacturer's instructions which may require proof that it has been carried out accordingly.
- (iii) Adding or attaching of ducting for cooling purposes using components supplied by the manufacturer or in compliance with the manufacturer's instructions. Adding or attaching communication equipment using components supplied by the manufacturer or in compliance with the manufacturer's instructions.

Note: Decoration of helmets is potentially dangerous, and members are warned of the hazard of using paint on approved helmets. Paint, stickers and transfers, or surface treatments can react with helmet shell material and affect its protective capacity; therefore, where a manufacturer provides guidelines or restrictions on the painting or decoration of helmets, these must be followed, using only paint or surface treatment specified by them (air drying acrylic, polyurethane enamel or others) and preferably a painter having their approval. This is particularly important for injection- moulded shells which are not usually suitable for painting.

The shell being painted should be efficiently masked as paint penetrating the interior can affect the performance of the helmet liner. Paints requiring heat curing should not be used and any process should not exceed the maximum temperature of conditioning of the helmet in the standard to which it is approved.

(c) Frontal Head Restraints:

- (i) The drilling of holes in helmets for the fitment of FHR tether posts shall be undertaken only by the manufacturer of the helmet or an approved manufacturer's agent.
- (ii) Tether posts may be fitted to helmets that are pre-drilled in accordance with the FHR manufacturer's instructions.
- (iii) The manufacturer's instructions should also be consulted for any considerations on the use of stickers and transfers.

The manufacturer's instructions should also be consulted for any considerations on the use of stickers and transfers.

- (d) **Accidents and Damage:** Competitors are cautioned against using a helmet which has been damaged or involved in an accident. Helmets shall be deemed by the Event Officials to be in a suitable condition for motorsport use.

3.4 GOGGLES AND VISORS

- (a) Goggles or visors must be worn by drivers of open automobiles. The driver/co-driver of a convertible automobile with a permanent hard roof shall not be required to wear eye protection provided the driver/co-driver is not exposed to debris entering the cockpit.
- (b) Those with glass lenses of any kind are not acceptable. Lenses shall be of a plastic material, with high-impact resistance, satisfactory optical qualities and complying with Australian Standard Specification AS 1609-1981, BS4110Z or equivalent international standard (i.e. ECE 22-05, VESC-8 or ANSI Z.87.1). An FIA Helmet standard may mandate an FIA Visor.
- (c) Goggles must be configured so as to minimise the entry of dust into the eye from any angle and be positively retained by an elastic strap behind the head or helmet. Conventional-style glasses are not acceptable.

3.5 HEARING PROTECTION

It is recommended that hearing protection devices (ear plugs) be worn at all times when exposed to high levels of noise.

3.6 PIT CREW APPAREL – CIRCUIT RACES

- (a) Each person working on automobiles in the pit or paddock area must at least wear shoes and socks, short pants and a short sleeved shirt. On race day, each pit crew must be neatly attired. The Organiser is authorised to refuse entry to the pit area of any person unsuitably attired.
- (b) In a circuit race where a refuelling operation is permitted, each person must at least wear shoes and socks, long pants and a short sleeved shirt. Each person involved in a refuelling operation or who is working within one metre of the refuelling or venting point must be attired in the following:
 - (i) One-piece overalls of flame-resistant materials extending from neck to wrists to ankles. Where such overalls consist of a single layer of material, separate flame-resistant underwear extending from neck to wrists to ankles must also be worn.
 - (ii) A flame-resistant balaclava which covers the entire head and neck save for one or two eye openings, unless a helmet providing a similar level of protection is worn.
 - (iii) Goggles which cover all exposed areas of skin not covered by the balaclava, or a full-face helmet and visor.
 - (iv) Shoes of leather or other flame-resistant materials which completely cover the feet and which fit closely around the ankle to minimise the ingress of spilt fuel. The soles must be resistant to fuel.
 - (v) Those directly handling fuel shall require socks and gloves which comply with the requirements of the FIA 1986 or 8856-2000 or 8856-2018 standard.
- (c) It is highly recommended that each person involved in work that is required for a pit stop in a circuit race wear a helmet which complies with a relevant standard suited to pit stop work in motorsport.

3.7 SUPERKART RACES AND PRACTICE

A superkart driver may wear appropriate wet weather clothing in addition to that specified.

3.8 MORATORIUMS

There shall be two years' notice on the implementation of any increased apparel requirements, save for any identified and pressing safety related requirements which may arise or become evident for non-international events.

SCHEDULE E – WHEELS AND TYRES

The following requirements and parameters apply to each automobile.

1. RIM WIDTH

- (a) The maximum rim width may be prescribed in category or class regulations.
- (b) If a steel centre is used, the following restrictions apply subject to the maximum specified in the category or class regulations:
 - (i) Racing Cars of the 1st Category and Sports Cars (Group 2A and 2C):
 - (A) Up to and including 2000cc capacity: 8.5".
 - (B) Over 2000cc capacity: 10".
 - (ii) Other automobiles of the 2nd Category and Touring Cars of the 3rd Category:
 - (A) When a steel wheel centre supplied by an automobile manufacturer as original automobile equipment is used, any rim attached to such centre shall not be more than 2" wider than the rim originally fitted to such centre.
 - (B) When a steel wheel centre other than that supplied by an automobile manufacturer as original automobile equipment is used, no widening is permitted.

2. TYRES

- (a) Tyre fitment shall be in accordance with the Tyre and Rim Association Manual; otherwise the tyre manufacturer's specifications shall apply.
- (b) Tread wear indicators as provided by the tyre manufacturer shall be the definitive indicator of tread depth.
- (c) Prior to practice or racing, each tyre must have tread in excess of the wear indicator save on the shoulder where localised wear may occur.

3. WET WEATHER TYRES

In wet conditions and with the approval of the Stewards, the Clerk of the Course may declare "untreaded tyres are not to be used at the commencement of the competition".

4. CONTROL TYRES

A Control Tyre is the tyre specified for a particular Group, Category, class, competition etc. and shall not be modified other than by the reduction of tread depth.

5. TYRE PRESSURE CONTROL VALVES

Any control valves that automatically manage tyre pressure are forbidden.

6. VALVE CAPS

Each tyre valve shall be fitted with a cap which effectively prevents leakage in use.

7. WHEEL SPACERS

A maximum of one metallic spacer may be used behind each wheel.

8. PRODUCTION CAR TYRE LIST

8.1 FOR ALL EVENTS

Manufacturer	Tyre
Achilles	123S
Bridgestone	RE 540S, RE55
Continental	Conti Competition C1, Conti Force Contact ZR 19 (front and rear variants)
DMACK	DMT-RC
Dunlop	Formula R (D83J, D84J, D93J, D01J, DZ02G, DZ03G), Formula 901, Formula W10, SP Super Sport Race
Federal	595 RSR, 595 RSR-R, FZ201, FZ202 R Spec
Hoosier	Street TD, Hoosier T.D.R., Speedster, D.O.T. Radial H20, Tarmac Rally
Hankook	Ventus TDZ221, RS-3
Kumho	Ecsta V700, V70A
Michelin	Pilot Sport Cup, TB15
MRF	ZTD2, ZTW2, ZST, ZTR, ZTTc
Nankang	AR1
Ohtsu	Falken Azenis, Azenis RT215
Pirelli	P Zero C, P Zero Corsa
Silverstone	FTZ Sport Type RR, S575, S585
Toyo	Proxes RA-1, Proxes R888, Trampion R881
Yokohama	A021R, A032R, A038R, A039R, A048R, A050

- (a) New tyres to be added to this list shall be generally and commercially available from stocks normally kept within Australia.
- (b) Motorsport Australia reserves the right to add or remove tyres from this list at any time.
- (c) To be considered for addition to the Production Car Tyre List, each tyre must have a moulded tread pattern across the entire width of the working area of the tyre. Such tread shall incorporate a void ratio of a minimum 10% of the total contact surface in the unworn tyre, and at least 5% when the tyre is worn to the tread wear indicators.

8.2. FOR SPEED EVENTS

In addition to the tyres listed above:

- (a) Any tyre that is certified and marked on the tyre wall with E-Mark, D.O.T or AS standards shall be acceptable for use in Speed events, provided the tyre is compliant with the requirements of Article 8.1(c); or
- (b) Tyres not containing E Mark, D.O.T or AS standard markings may be accepted by Motorsport Australia for use at Speed events. Such tyres shall be generally and commercially available from stocks normally kept within Australia and listed below. Motorsport Australia reserves the right to add or remove tyres from the below list at any time.

Manufacturer	Tyre
Avon	ACB10 Semi slick compound cross-ply
American Racer	M28 Compound 704

- (c) To be considered for addition to the list for use at Speed Events, each tyre must have a tread pattern across the entire width of the working area of the tyre. Such tread shall incorporate a void ratio of a minimum 10% of the total contact surface in the unworn tyre.

SCHEDULE F – AEROFOILS

The following are the Motorsport Australia parameters regarding the fitting of aerofoils and other aerodynamic devices to automobiles.

1. GENERAL

For each automobile (1st, 2nd, 3rd, 4th, 5th and 6th categories) coachwork shall be deemed to include all external parts of the automobile which extend above the highest point of either the front or rear complete wheels (with tyres) with the exception of units definitely associated with the functioning of the engine or transmission and the safety cage structure.

Any specific part of the automobile which has an aerodynamic influence on the stability of the automobile must be mounted on the entirely sprung part of the automobile and shall be firmly fixed whilst the automobile is in motion.

Neither the safety cage structure nor any of the units associated with the functioning of the engine or transmission shall have an aerodynamic effect by creating vertical thrust.

The leading edge of any aerofoil fixed to the front of the automobile shall not be sharp.

Switches for battery isolation and firefighting equipment may project beyond the coachwork without infringing regulations.

2. 2ND CATEGORY

Each automobile shall comply with the following requirements (except for an automobile which complies with Group 2C Regulations):

The highest point of any forward facing gap in the coachwork shall not be situated above a horizontal plane 800mm above the lowest point of the entirely sprung structure of the automobile. The maximum width of coachwork shall not exceed by more than 200mm the maximum width between the two vertical planes tangent to the outer faces of the front/rear wheels.

3. 1ST CATEGORY

Each automobile shall comply with the following requirements:

- (a) No element of coachwork may exceed in height a horizontal plane situated at 900mm above the ground. Neither the safety cage structure nor any of the units associated with the functioning of the engine shall be included. Measurements are to be taken with the driver on board.
- (b) Each automobile of a type registered at 1 January 1975, but constructed after 1 July 1975; and each automobile of a type not registered at 1 January 1975, but constructed after 1 January 1976; and each automobile registered at 1 January 1975, but which subsequently varied; must all comply with the following requirements:

	F4000	F2	F/Ford
Maximum width ahead of front wheels	1,500mm	1,500mm	950mm
Maximum width ahead of front wheels, above height of wheel rims	1,100mm	1,100mm	950mm
Maximum width between front and rear wheels + deformable	1,300mm	1,100mm +200mm	1,300mm
Maximum width behind rear wheels	1,100mm	1,100mm	1,100mm
Maximum front overhang		1,000mm	
Maximum rear overhang (from centre of wheel/axle)	800mm	1,000mm	

- (c) Each wheel shall be external to the coachwork.
- (d) Unless otherwise specified in technical regulations the coachwork opening giving access to the cockpit must be at least 600mm long; and 450mm wide, maintained over 300mm from the rearward point of the seat backrest towards the front. It must be able to be entered or left without it being necessary to open a door or remove a panel. Sitting at their steering wheel the driver must be facing forwards. Moreover, the cockpit must be so conceived that the maximum time necessary for the driver to get out does not exceed five seconds.

SCHEDULE G – FUEL

1. GENERAL

- (a) All fuel used in competition must comply with the prescriptions of this Schedule. All fuel must be used without additives other than those permitted herein. Other than for pump fuel, the mixing of fuels from different oil companies, or of different grades and/or types of fuel from the same oil company is forbidden.

An oil company shall be deemed to be either:

- (i) a company with oil refining capacity either in Australia or Internationally;
 - (ii) a company with roadside retail fuel bowser outlets within Australia; or
 - (iii) a marketing company with national distribution capability, and specifically recognised by Motorsport Australia.
- (b) Each Group/Classification or category or event regulation/s must detail the fuel which is permitted in accordance with this Schedule.
- (c) It is not permitted to use a fuel of any type that contains lead in any form.

2. COMMERCIAL FUEL

A petrol, automotive diesel or liquefied petroleum gas (LPG) or ethanol blended fuel, e.g. "E85" (as defined below) produced by an oil company and available for commercial sale in all States and mainland Territories of Australia.

With the exception of Ethanol Blended Fuels, all Commercial Fuel shall comply with the Fuel Standards Determinations made under Section 21 of the Fuel Quality Standards Act (2000).

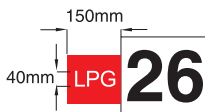
Fuel which is the subject of Approvals made under Section 13 of the Fuel Quality Standards Act (2000) shall not be regarded as Commercial Fuel.

2.1 PUMP FUEL

A Commercial Fuel (as defined above), with a maximum ethanol content of 10%. Pump Fuel shall be available for sale on demand from a roadside retail bowser outlet at each of at least five separate service stations in each of at least three Australian States. A mixture of Pump Fuels is permitted (e.g. brands of unleaded petrol [ULP] may be mixed; 10% ethanol-blended fuels and ULP may not be mixed).

2.2 LIQUEFIED PETROLEUM GAS:

A commercial LPG being either butane, propane or a mixture of both, and of a type which is dispensed from a roadside retail bowser. Where an automobile utilises LPG, a sign in the form of the white letters LPG on a red reflective background, as per drawing G-1, must be placed immediately to the left and centralised vertically to all competition numbers.



Drawing G-1

Note: Unless specifically approved otherwise by Motorsport Australia, LPG must not be used in circuit races involving re-fuelling.

2.3 DIESEL

Diesel fuel must be gas oil corresponding to the following specifications:

- (a) hydrocarbon level, 99.0 % by weight minimum
- (b) specific gravity, 860 kg/m³ maximum
- (c) cetane number 55 maximum (ASTM D 613)
- (d) calculated cetane number 55 maximum (ASTM D 976-80).

2.4 ETHANOL BLENDED FUEL

Ethanol Blended Fuel is defined as only containing the following constituents:

- (a) Anhydrous fuel grade ethanol (between 70% and 85% \pm 5% v/v)
- (b) Unleaded petrol (15% and 30% \pm 5% v/v)
- (c) Corrosion inhibitor (optional)
- (d) Colouring dye (optional)
- (e) Other constituents (0.2% max v/v)

3. RACING FUEL

3.1 UNLEADED RACING FUEL

Unleaded racing fuel is defined as an unleaded petrol produced by an oil company in compliance with the specifications detailed in FIA Article 252 of Appendix J to the current FIA Year Book. Such Unleaded Racing Fuel supplied from a drum is permissible.

NOTE: Specific Group regulations (i.e. 5th Category Historic) may permit the use of an alternate Unleaded Racing Fuel.

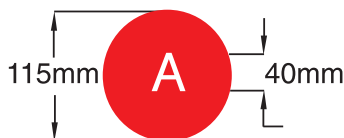
4. ALCOHOL

Alcohol fuels may only be used where specified for 5th Category automobiles or where a specific Group regulation permits its use.

- 4.1 For other than 5th Category automobiles, such fuels must contain a minimum 95% V/V of methanol and/or ethanol and a maximum 5% ketone content. Nitromethane is expressly prohibited.

- 4.2 For individual automobiles of the 5th Category, specific alcohol based fuels with a constitution other than as described in 4.1 above may be approved; the constitution of such fuels shall be listed on the Certificate of Description of the particular automobile in question and must be used unadulterated in that automobile.

- 4.3 In each such case, it is mandatory that an automobile using alcohol fuel shall carry a symbol in the form of the letter "A" in white on a red circle of approximately 115mm diameter as shown in drawing G-2 below. This symbol must be placed adjacent to the competition number on each side of the automobile, and the filling cap of the fuel tank.



Drawing G-2

5. BIO FUELS

A petrol, diesel or alcohol fuel produced exclusively from sustainable resources. Where such a fuel does not meet the requirements of Articles 2, 3 or 4 above, its use must be specifically authorised by the Fuels Regulation Committee.

6. OXIDANTS

Only air may be mixed with the fuel as an oxidant. The use of Nitrous Oxide must be approved by Motorsport Australia and included in event specific regulations.

7. ADDITIVES

- 7.1 Any substance, other than air, incorporated into the fuel subsequent to its final blending by the producing oil company is deemed to be an additive. Nothing in the foregoing shall be deemed to prohibit the addition of water, an approved lead replacement additive or a lubricant provided that such additive does not increase the octane or cetane rating, oxygen content or specific heat content of the fuel.

Specific Group regulations (i.e. 5th Category Historic) may permit the use of alternate additives.

List of Approved Lead Replacement Additives:

- (i) Valvemaster®,
- (ii) Redline Lead Substitute®,
- (iii) Penrite Valve Shield®,
- (iv) PM 800 Fuel System Conditioner®,
- (v) Elf Millesim®.

- 7.2 Ethanol-blended fuel may utilise a fuel stabiliser and corrosion inhibitor provided the same conditions apply as Article 7.1.

8. FUELS REGULATION COMMITTEE

8.1 APPOINTMENT

The Fuels Regulation Committee has been established by the Board of Motorsport Australia for the purpose of considering the application of restrictions to fuel usage. The Chairman and Committee members shall be appointed annually by the CEO.

8.2 APPLICATION FOR RESTRICTIONS

Requests for restrictions to fuel usage may only be addressed to the Committee by:

- (a) Sporting Commissions of Motorsport Australia;
- (b) The holders of valid Activity, Series or Championship Management contracts;
- (c) Persons or organisations recognised by Motorsport Australia as representing particular Groups of automobiles;
- (d) Event organisers.

8.3 CRITERIA FOR CONSIDERATION OF RESTRICTIONS TO FUEL:

The Committee will consider the following criteria in making their recommendation:

- (a) The nature and level of the competition: Is the competition at amateur or professional level, and will the type of fuel play a major role in the outcome of the competition?
- (b) Applicability of Government regulations: Are there statutory restrictions that would affect the fuel used in competition?
- (c) Availability of fuel: Is there a particular supply issue affecting the competition?
- (d) Technical appropriateness of fuel: Does the nature of the fuel restriction sought match the technical specifications of the automobiles?
- (e) Cost containment: Is the request for a restriction for fuel, based on cost containment, consistent with other cost containment philosophies of the competition?
- (f) Health and safety: What effect will any restriction have on OH&S issues?
- (g) Performance criteria: What effect will a restriction have on performance relativities within the competition?
- (h) Commercial considerations: What commercial arrangements may form part of any restriction to fuel (e.g., sponsorship, rebates, promotion etc.)?

Further information on these criteria is available from the Executive Officer, Fuels Regulation Committee, at the Motorsport Australia National Office.

8.4 RESTRICTIONS:

The Committee will consider requests for restrictions to:

- (a) Nominated Suppliers: In general, a "Control Fuel", being a restriction to a particular brand and/or type of fuel, will not be authorised. Notwithstanding the above, a contract between the competition organisers and a particular fuel supplier may be recognised provided that such a contract is the outcome of an open tender process. Where such a fuel is supplied to the competitors it is prohibited to alter the composition of the fuel in any manner, including by the use of additives otherwise permitted under article 7.

- (b) Fuel supplied by the Organisers: A restriction may be sought to limit fuel used by each competing vehicle at an event to "Fuel supplied by the Event Organisers". Such a restriction will not be granted unless the organisers can demonstrate that they have taken reasonable steps to ensure that appropriate grades of fuel are available to suit all competing groups of automobiles.
- (c) Commercial Fuel: as defined above.
- (d) Pump Fuel: Restrictions to pump fuel will be obtainable to a limited number of competitions that can strictly satisfy all the criteria of article 8.3 above. A restriction to pump fuel will not be authorised in conjunction with any other type of fuel.
- (e) Ethanol Blended Fuel: as defined above.
- (f) Unleaded Racing Fuel: as defined above.

9. FUEL TESTING

Fuel samples may be drawn for testing from a competing automobile at any time during the period of time from the commencement of the Event until the vehicle is released from Parc Fermé at the conclusion of the event, should one be organised.

It is the competitor's responsibility to provide the means by which fuel samples may be taken from the automobile; the method being subject to the approval of the Chief Scrutineer. Sampling requiring the disconnection of hoses containing fuel under pressure is not acceptable.

Whilst the fuel samples for testing are being taken, the Competitor, or their nominated representative must be in immediate attendance to observe the process. Where the Competitor or their nominated representative cannot be present within a reasonable time, the Chief Scrutineer must notify the Stewards, who shall appoint a proxy observer, being an Official of the event, who shall act as the nominated representative of the Competitor.

For all National Championships and Series, and other events as specified in event regulations, the Competitor must declare to the Scrutineers, at the time of sampling, the brand and type of fuel that is in the automobile's fuel tank. Any additive, including lubricants, must also be disclosed to the scrutineers. Where a permitted mixture of fuels is present, all component fuels must be declared.

Samples shall be tested according to procedures A or B below.

Test Procedure A: Testing at the Event

The Chief Scrutineer may choose to test fuel samples at the event. To this effect, one sample of fuel may be taken for testing under the conditions outlined above from each or any competing automobile. The Competitor may, at their discretion, request a second sample be drawn at the same time. After being duly identified and sealed, this second sample may be retained by the Competitor*.

Testing at the event shall be limited to:

- (a) physical observation of the sample (colour, opacity)
- (b) testing using whatever specialist equipment is available at the event (e.g. electrical conductivity, density, gas chromatography etc.)

Where a charge is raised as a result of such testing, the Chief Scrutineer or their representative shall give evidence at the subsequent Stewards Hearing or Inquiry, although they shall not be accorded the status of "Judge of Fact". Notwithstanding this, the Stewards shall be obliged to take into consideration any evidence thus presented.

***The Competitor may use the retained sample in their defence provided that the seal of the sample retained by the competitor is broken in the presence of the Stewards. Where the Scrutineer deems that no action is necessary, the container holding the Competitor's sample shall be returned by the Competitor to the Scrutineers upon request.*

Test Procedure B: Testing by a Motorsport Australia approved Laboratory

Motorsport Australia shall maintain a list of approved laboratories for the testing of fuel.

For tests by an approved laboratory, two fuel samples shall be drawn and sealed into identified containers. The seal on each container shall be affixed in such a way as to ensure the rupture of the seal upon the opening of the container. Each seal shall bear identification of the event, the name and signature of the Scrutineer taking the sample and the name and signature of the Competitor. The samples (Samples A and B) shall then be sent to Motorsport Australia, which shall send Sample A to a Motorsport Australia approved laboratory. The determination of fuel type and composition shall be by comparison against a reference library of results for known fuel types determined by the method ASTM D-3710-95 (or equivalent).

Where the approved laboratory notifies Motorsport Australia that Sample A has been found to be not in conformity with the prescriptions contained herein, Motorsport Australia shall lodge sealed fuel Sample B with the same approved laboratory. Where the results of the second test Sample B corroborate the initial determination of Sample A, the fuel shall be deemed to be not in conformity with the prescriptions of the present Schedule. This finding shall be binding on any Stewards' Hearing, Appeal Tribunal or any subsequent AMSAC Hearing. Where a discrepancy exists between the results of Samples A and B, no action shall be taken against the Competitor.

Note: There are costs associated with fuel testing by a Motorsport Australia approved laboratory. Prior to any testing of this nature the Motorsport Australia Technical Department must be advised to inform on costs.

There are special conditions associated with the transport and handling of flammable liquids. Contact Motorsport Australia to ascertain an appropriate method of transporting the fuel.

10. HEALTH WARNING

All participants in motorsport are reminded that fuels, oils, lubricants and coolants are highly specialised substances. Apart from the ever-present risk of fire, participants must be aware that these agents may contain substances that are extremely dangerous to one's health if misused, inhaled or allowed into contact with human skin. Some of the components of these fuels, oils and lubricants are suspected of having the potential to cause cancer in rare instances. The use of petrol as a general cleaning and washing agent is a common misuse of a potentially dangerous substance.

SCHEDULE H – FIRE EXTINGUISHERS

(not applicable to Superkarts)

1. GENERAL

Each automobile in any competition other than Non-Speed or Race events, except where noted in specific Group/category/class regulations, must be equipped with a fire extinguisher which complies with the following conditions.

1.1 HAND-HELD FIRE EXTINGUISHERS:

- (a) Each hand-held fire extinguisher shall be secured using a metal bracket attached to the automobile with only high tensile bolts or equivalent fasteners and/or sufficient clamp/s and must remain restrained under a deceleration or acceleration of 25g; and
- (b) Must be capable of removal by the driver (or crew, where applicable) while seated in their normal respective position for competition with safety harness unfastened, unless varied by specific category regulations and without the aid of tools.

1.2 PLUMBED-IN FIRE EXTINGUISHERS:

Each plumbed-in fire extinguisher shall be fitted in accordance with FIA Appendix J, Article 253 and with attention to the following:

- (a) Each extinguisher container shall be adequately protected and may be situated within the cockpit or luggage compartment. If situated in the luggage compartment it shall be at least 300 mm from the outer edge of the bodywork in all horizontal directions.
- (b) The container must be secured by a minimum of 2 screw-fastened metallic straps and the securing system must be able to withstand a deceleration of 25g.
- (c) Anti-torpedo tabs must be fitted.

2. SERVICE AND INSPECTION

- (a) Each hand-held extinguisher shall be inspected by a scrutineer at targeted scrutiny, or otherwise prior to competition. This inspection shall involve visually checking the unit and its mountings for damage and corrosion, checking the pressure of the contents via an installed gauge, and inverting and shaking the container to check for settling of the extinguishant. Where practical, it is recommended the extinguisher be weighed (mandatory where no gauge is installed).

The following information shall be visible on each extinguisher:

- (i) capacity;
- (ii) type of extinguishant;
- (iii) weight or volume of the extinguishant; and
- (iv) date of manufacture of the extinguisher.
- (b) Each AS1841 standard fire extinguisher shall be serviced every three years in accordance with AS1851 for a fire extinguisher in an adverse operating environment. If the extinguisher is compliant with another acceptable standard, it shall be serviced every two years. This servicing shall be undertaken in accordance with the procedures laid out by the manufacturer and/or the relevant standard (e.g. AS1851 for AS1841 extinguishers) and shall include a hydrostatic pressure test of the extinguisher body. A report from the servicing agent shall be supplied to a scrutineer on demand as proof of a service being completed. A service tag shall not be accepted as proof of the extinguisher having been serviced.
- (c) Each fire extinguisher which is homologated by the FIA shall be serviced either by the manufacturer or their agent no more than two years after either the date of filling or the date of the last service. This servicing shall be undertaken in accordance with the procedures laid out by the manufacturer and/or the relevant standard and shall include a hydrostatic pressure test of the extinguisher body.

3. SPEED EVENTS, DRIFTING AND RACE EVENTS

Each automobile must be fitted with a fire extinguisher(s) in accordance with Table H-1.

Where a Regularity or Sprint Speed event is conducted within a Race event the requirements for a Fire Extinguisher shall be as for Race in accordance with Table H-1. In this application the track must maintain the required fire response as applied for the Race event for the Speed event component.

The fitment of an "on-board" extinguishing system homologated by the FIA shall be an acceptable alternative to the fitment of a separate extinguisher.

4. RALLY/ROAD EVENTS

Each automobile shall be fitted with a hand held fire extinguisher/s in accordance with Table H-1, and in addition shall be equipped as follows:

- (a) In an International event each automobile shall comply with the current FIA ISC for fire extinguishers.
- (b) In an Australian Rally Championship event each automobile in the ARC Outright class/es shall be equipped with a plumbed-in fire extinguisher system in accordance with those permitted for Rally use in FIA Technical List No. 16 and FIA Technical List No. 52 and fitted in accordance with FIA Article 253. However where a driver is competing in their home State² in an automobile manufactured prior to January 2000, the automobile will be exempt from this requirement.
- (c) In a State Championship event an automobile manufactured after 1 January 2000³ shall be equipped with a plumbed-in fire extinguisher system approved by the FIA¹.

Notes

¹ Plumbed-in fire extinguisher systems that are in accordance with those permitted for Rally use in FIA Technical List No. 16 and FIA Technical List No. 52 and fitted in accordance with FIA Article 253.

It is highly recommended that each automobile in a State Championship or lower status event is also fitted with a compliant FIA extinguisher system.

² Where in a particular year there is no round of the ARC in the driver's home state, a driver may nominate another State to be their home State.

³ This date refers to the original date of manufacture of the automobile, and not the date on which it was built as a rally automobile.

- (d) For each type of rally/road event, refer Table H-1.

Note: It is highly recommended that each new automobile built for Rally/Road is equipped with an extinguisher system compliant, for Rally, with FIA Technical List No.16 or FIA Technical List No. 52.

5. OFF ROAD EVENTS

Each automobile must be fitted with a fire extinguisher/s in accordance with Table H-1.

Table H-1

Event type	Hand held extinguisher standard	Minimum total capacity	Number of extinguishers required
Speed, Drifting	AS1841	900g	1 or more
Off Road	Except AS1841.2	1.8kg	
Special Stage Rally/Rally Sprint S2	AS1841.5	2.0kg	1 or 2
	AFFF or other extinguishant permitted by the FIA	2.4L @ 12.0 bar (or as otherwise determined by the FIA)	
Rally Sprint S1 (only if venue is fully manned)	AS1841.5	1.0kg	1 or more
Race	Not mandatory*	Not mandatory*	Not mandatory*
Electric Vehicles	AS1841.5	2.0kg	1 or 2

* Fire extinguishers are not mandatory for an automobile competing in Race events, except where mandated by Group/category/class regulations, however a fire extinguisher that meets the standards for Speed events as above is highly recommended.

Note: Halon extinguishers (1201 or 1311, including BCF) are not permitted under civil legislation.

SCHEDULE I – SAFETY HARNESSES AND WINDOW NETS

(not applicable to Superkarts)

1. SAFETY HARNESS GENERAL REQUIREMENTS

- (a) A safety harness (including a seat belt) shall be compliant with a Standard as specified below and be fitted and worn in accordance with the manufacturer's directions, with Tables I-1 and I-2 of this Schedule and any additional requirement imposed by specific category, group and/or supplementary regulations.
- (b) Each safety harness shall comply at least with one of the Standards as specified in Table I-1 below.
- (c) Harnesses of a higher level than specified are permitted and encouraged.
- (d) Each safety harness with the words "For FHR use only" which appears on each shoulder strap shall be worn only in conjunction with a FHR device.
- (e) Any international event listed on the FIA International Calendar will be subject to the safety harness standards detailed in the FIA ISC.

Important note:

- (i) Some safety harnesses may not comply with the law. Where the automobile is to be driven on a public road, it is the competitor's responsibility to ensure that it complies with the law.
- (ii) A safety harness damaged in any way, including in a collision, shall be subject to inspection by a scrutineer. If appropriate, the automobile's log book shall be endorsed with a requirement that the belt/harness be replaced.
- (f) The fitment of an elastic cord and or any retention device not homologated by the FIA, which is bonded or sewn to a Safety Harness shoulder strap is not permitted.
 - (i) It is permitted to use a Velcro® piece or alternative to retain the shoulder strap away from the driver during a driver change, provided this does not apply a load or crush to the shoulder strap and its webbing when fitted to restrain the driver.
 - (ii) It is permitted to attach an elastic cord to the waist strap only. Attachment shall be to the metal buckle only.

2. SAFETY HARNESS MOUNTINGS

- (a) A safety harness shall be securely mounted on at least two points (Type D), three points (Types B and C) or five points (Type A) in compliance with the prescriptions of Drawings I-1, I-2 and I-3. If the two shoulder straps (Types B and C) join prior to a common mounting point then that junction shall be at least 150mm behind the wearer's neck. Under no circumstances shall a safety harness mounting bolt be used to affix a safety cage to the bodyshell.
- (b) A safety harness shall be installed in accordance with the manufacturer's instructions with consideration to the requirements when using a Frontal Head Restraint and application of the following:
 - (i) The shoulder straps shall be directed to the rear and installed in such a way that they do not make an angle greater than 45° to the horizontal from the occupant's shoulder where a frontal head restraint is not used. It is highly recommended that this angle should not exceed 10° (refer drawing I-1).
 - (ii) The maximum angles in relation to the centre-line of the seat are 20° divergent or convergent (refer drawing I-2). The shoulder straps may be installed crosswise symmetrically about the centre-line of the front seat mounting points for a safety harness.
- (c) A safety harness shall be mounted using the following:
 - (i) On a series production automobile, any unmodified seat belt mounting point may be used;
 - (ii) Where a safety harness is affixed to an un-reinforced section of the body shell, each attachment point shall be reinforced by the use of a plate not less than 75mm x 50mm x 3mm thick (refer drawing I-4);
 - (iii) Except for a crutch strap mounted in accordance with (d) any bolt used shall be a minimum of 10mm grade 8.8, or an eye bolt to the recognised thread diameter of 7/16" or 11mm except for homologated 1st category applications;
 - (iv) Shoulder straps may be fixed to the safety cage or to a reinforcement bar by means of a loop, and/or be fixed to a transverse reinforcement compliant with Schedule J and the following:
 - (A) When looped around a transverse bar adjustment mounting buckles are to be placed as close as possible to the bar to reduce the amount of slip of the shoulder strap mountings.
 - (B) It is permitted to retain a shoulder strap/s into position to maintain FHR adjustment using material such as safety cage padding.
- (d) Only a crutch strap or straps may be mounted in accordance with drawing I-6 where the following shall apply:
 - (i) Bar/s shall not bend under a strap load of at least 14.7kN
 - (ii) All edges shall be appropriately rounded (>1.5mm radius)
 - (iii) The bars shall directly clamp on each other firmly clamping the webbing
 - (iv) Each attachment point shall be reinforced by the use of a plate in accordance with drawing I-4 or a single plate in accordance with drawing I-5
 - (v) The belt is correctly routed in accordance with drawing I-6

3. FITMENT OF SAFETY HARNESSES FOR FRONTAL HEAD RESTRAINT (FHR)











- (a) Each safety harness must be compliant with FIA or SFI standards and it is strongly recommended to use only a 6 point harness homologated to FIA standard 8853/98 or FIA standard 8853-2016. A safety harness with either a 75mm or a 50mm wide shoulder strap may be used with FHR. The following shall apply:
 - (i) The length adjustment device of the shoulder strap shall be positioned on the FHR yoke with the upper edge not more than 70mm from the lower edge of the FHR yoke as shown in Drawing I-7.
 - (ii) The shoulder strap anchorage points on the automobile shall be symmetrical about the centre line of the driver's seat. When viewed from above, the angle between the shoulder straps shall be approximately 20°-25° as shown in Drawing I-8.
 - (iii) This can be achieved with reference to the values in Table I-3 which have been calculated based on 75mm wide belts (values for 50mm wide belts are shown in brackets) and four FHR collar sizes according to Drawing I-10. Negative values indicate that the shoulder straps are crossed. These values should be closely respected, but a tolerance of +/-20 mm would be acceptable. Strap movement in the anchorages should be taken into account.
 - (iv) The values in red (underlined) denote that theoretical separation is less than strap width. In this case it is recommended that the straps are installed side by side to avoid any overlap, hence the actual separation shall be equal to the strap width. If the value is negative, the strap should be crossed. Shoulder straps over 200mm long are not recommended.

4. WINDOW NETS

- (a) In a circuit race, each closed automobile which is required to have a safety cage fitted shall have a safety window net fitted in the driver's door window opening.
 - (i) The window net must cover the opening forward to the centre of the steering wheel and be able to withstand a load of 500N applied at any point.
 - (ii) The net may be locally modified to preserve the driver's view of the external mirror.
 - (iii) The net must be affixed by means of a rapid release system so that, even with the automobile inverted it must be possible to detach the mechanism with one hand.
 - (iv) The handle or lever must have coloured markings.
 - (v) A push button release system is authorised provided that it respects the prescriptions of this article. The push button must be visible from the outside, be of a contrasting colour and be marked "press".
- (b) On each automobile derived from series production automobiles manufactured after 1970 and which retains the unmodified door, hinges and latches of the registrable automobile, the net may be mounted to the door frame.
 - (i) A method of permanent attachment (metal strip with bolts or rivets) must be used to affix the net to the door and shall incorporate a quick release system.
 - (ii) Such an automobile fitted with a permanently closed shatterproof window on the driver's door that complies with strength requirements imposed above will be deemed to comply with the requirement for a window net.
 - (iii) On each other automobile the net must be mounted to the safety cage.

Note: Each 5th Category automobile, when competing in an event exclusively for such an automobile, is exempt from the requirement for Window Nets.
- (c) Each Off Road automobile, must be equipped (for each occupant) with a safety net or arm restraints in accordance with the following.
 - (i) Each device must ensure that each arm of each crew member cannot project beyond the line of the bodywork of the automobile.
 - (ii) Where an occupant is not using arm restraints a window net must be used to cover each opening, except for the front window opening.
 - (iii) For an automobile with an opening roof each occupant shall be required to have a wrist/arm restraint on each arm.
 - (iv) If arm restraints are used they must be worn by each occupant at all times whilst the automobile is moving in competition. A wrist restraint must be at least to an SFI 3.3 standard.

Table I-1

Type	Configuration	Acceptable standards	Identification
A	6-Point Harness 	FIA 8853-2016³ FIA Hologram compulsory	
	5-Point Harness 	FIA 8853/98¹ FIA Hologram compulsory for each harness manufactured after 01/01/2013 SFI 16.1²	
B	4-Point Harness 	Includes Level A FIA 8854/98¹ AS 2596 ECE R16	
	3-Point Harness 		
C	Lap Sash Belt 	AS 2596 ECE R16 AS E35	
D	Lap Belt 	AS 2596 ECE R16 AS E35	

Notes

¹ "Not valid after XXXX" shown on each strap as detailed below:

- (i) For International events, safety harnesses must not be used after 31 December of the year stated (XXXX).
 - (ii) For all other events, safety harnesses must not be used after 31 December, five years after the year stated (XXXX).
- The extension of the safety harness validity detailed above for non – international events is subject to the following conditions:
- (iii) Safety harnesses must be inspected during the normal scrutiny process;
 - (iv) Each competitor must inspect and replace any damaged or worn safety harness before any competition as required.

² SFI Harness to be returned to original manufacturer for re-webbing within two years of the date of manufacture shown on SFI label or be replaced. This requirement is imposed by the SFI Foundation (Inc).

³ FIA 8853-2016 only in accordance with FIA Presentation Forms: <https://www.fia.com/presentation-forms-harnesses-according-fia-standard-8853-2016>

Table I-2

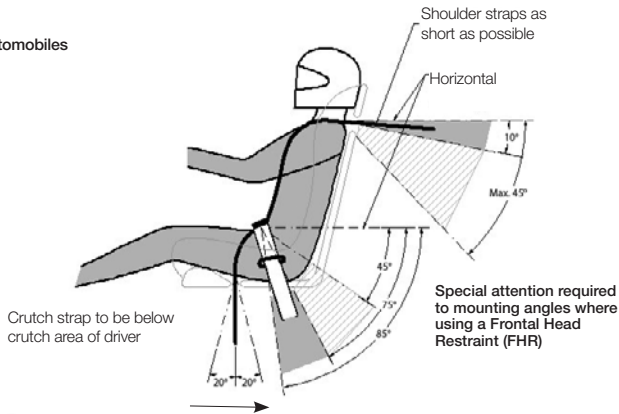
Event type	Event Permit level	Type	Notes
Observed Section Trial	All	D	
Motorkhana	All	D	
Khanacross	All	C	"Specials" only: Type B minimum
Speed Events	All	C	Registered closed automobiles
		B	Other automobiles
		A	Where FHR is required – refer Schedule D
Races ¹ - 1st Category Group 2A/2C	All	A	
Races ¹ - Other automobiles	All	A	
Rally	All	A	
Rallysprint	S1	C	
	S2	A	
Other Road Events	Touring / Navigation Assemblies	C, D	Must comply with civil regulations or otherwise as required for any other sub event/s or special test/s.
	Touring Road	C, D	Must comply with civil regulations or otherwise as required for any other sub event/s or special test/s.
Off Road	All	A	

Notes

¹ Except 5th Category: For automobiles of the 5th Category whilst competing in events exclusively for the 5th Category, each safety harness shall be of a type and configuration as specified in the specific group technical regulations.

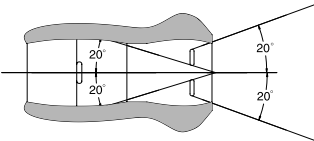
Drawing I-1

Not applicable to 1st Category Automobiles



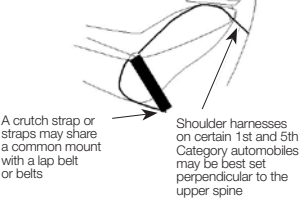
- Recommended range of angles for Safety Harness Belts
- Acceptable range of angles for Safety Harness Belts. Note that the angles are taken from the driver's body, not the slots in the seats

Drawing I-2

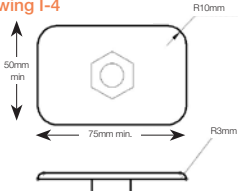


Drawing I-3

1st Category and similar automobiles



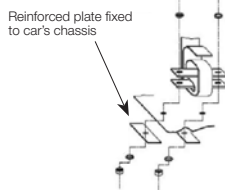
Drawing I-4



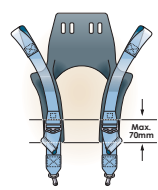
Drawing I-5



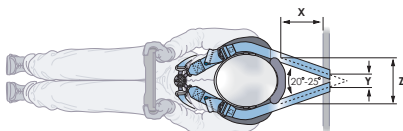
Drawing I-6



Drawing I-7

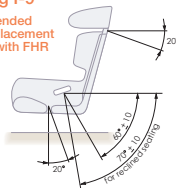


Drawing I-8



Drawing I-9

Recommended harness placement with use with FHR



Drawing I-10

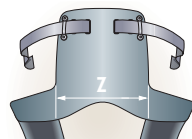


Table I-3

Table 1: Reference Values for 120mm FHR Collar

Z FHR COLLAR WIDTH (MM)	120							
X FHR to belt anchorage (mm)	100	200	300	400	500	600	700	800
Y belt anchorage to separation (mm)	135- (110)	95 (70)	55 (30)	15 (-10)	-25 (-50)	-65 (-90)	-105 (-130)	-145 (-170)

Table 2: Reference Values for 140mm FHR Collar

Z FHR COLLAR WIDTH (MM)	140							
X FHR to belt anchorage (mm)	100	200	300	400	500	600	700	800
Y belt anchorage to separation (mm)	155 (130)	115 (90)	75 (50)	35 (10)	-5 (-30)	-45 (-70)	-85 (-110)	-125 (-150)

Table 3: Reference Values for 160mm FHR Collar

Z FHR COLLAR WIDTH (MM)	160							
X FHR to belt anchorage (mm)	100	200	300	400	500	600	700	800
Y belt anchorage to separation (mm)	175 (150)	135 (110)	95 (70)	55 (30)	15 (-10)	-25 (-50)	-65 (-90)	-105 (-130)

Table 4: Reference Values for 180mm FHR Collar

Z FHR COLLAR WIDTH (MM)	180							
X FHR to belt anchorage (mm)	100	200	300	400	500	600	700	800
Y belt anchorage to separation (mm)	195 (170)	155 (130)	115 (90)	75 (50)	35 (10)	-5 (-30)	-45 (-70)	-85 (-110)

Definitions for the reference values:

- dimension Z (mm) = width of the FHR collar, as shown in Drawing I-8 and I-10
- dimension X (mm) = distance from the rear edge of the FHR-belt-bearing-surface to the automobile attachment point (mm) as shown in Drawing I-8
- dimension Y (mm) = separation of the centres of the two shoulder straps at the automobile attachment points (mm) as shown in Drawing I-8

Values calculated based on 75mm wide straps (values for 50mm wide straps are shown in brackets)

Values in **ORANGE** colour denote that theoretical separation is less than belt width. In this case it is recommended that the belts are installed side by side to avoid any overlap, hence the actual separation shall be equal to the belt width. If the value is negative, the belt straps should be crossed.

NOTE: Shoulder straps over 200mm long are permitted but not recommended.

SCHEDULE J – SAFETY CAGE STRUCTURES

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The online Motorsport Australia Manual is the most up to date source of Motorsport Australia motorsport rules and requirements, with amendments to the Manual clearly marked in the online PDF's.

1. GENERAL

- (a) Where the fitting of a safety cage structure is specified, it shall be constructed and installed in accordance with this Schedule.
- (b) Any repair to a safety cage structure shall comply with the requirements applicable at the time of original manufacture.
- (c) Any modification to a homologated or certified safety cage is forbidden without the express permission of Motorsport Australia. In the case of an FIA homologated or Motorsport Australia certified safety cage structure any repair shall be carried out by the manufacturer or with the manufacturer's approval. The repaired safety cage shall remain in compliance with the homologation VO or certificate.
- (d) Any tube forming part of a safety cage structure shall not carry fluids or any other materials.
- (e) The safety cage structure shall not unduly impede the entry or exit of the driver/crew.
- (f) Unless category regulations permit, complete parts of upholstery or trim shall not be removed, however the interior trim and dashboard may be modified locally (e.g. by cutting or distorting) in order to fit a safety cage structure. A rear seat may be folded down. A fuse box may be relocated to enable a safety cage structure to be fitted.
- (g) Supplementary Regulations may impose more stringent requirements than those specified in this Schedule.
- (h) If a vehicle is to incorporate a carbon fibre survival cell technology, refer to Motorsport Australia Technical Department for advice prior to purchase or building.

2. DEFINITIONS



Type 1



Type 2



Type 3



Type 3

2.1 SAFETY CAGE STRUCTURE

Multi-tubular structure installed in the cockpit and fitted close to the bodyshell, the function of which is to reduce the deformation of the bodyshell (chassis) in case of an impact.

2.2 ROLL BAR

A tubular frame with two mounting feet, forming a hoop.

2.3 MAIN ROLL BAR (DRAWINGS J-2 AND J-3)

A transversal and near-vertical (maximum angle $\pm 10^\circ$ to the vertical) single piece hoop located across the vehicle and immediately behind the front seat/s. The tube axis shall be within a single plane.

2.4 FRONT ROLL BAR (DRAWING J-3)

A roll bar, the shape of which follows the pillars and upper edge of the windscreen (where fitted).

2.5 LATERAL ROLL BAR (DRAWING J-4)

A near-longitudinal single piece hoop located along one side of the vehicle, the rear pillar part of which is near-vertical (maximum angle $\pm 10^\circ$ to the vertical) and straight in side view and located immediately behind the front seat/s, and the front part of which follows the windscreen pillar. Drawing J-4 may be replaced with that of drawing J-4b.

2.6 LATERAL HALF ROLL BAR (DRAWING J-2)

Similar to the lateral roll bar but without the vertical rear leg. Also referred to as a 'front leg'.

2.7 LONGITUDINAL MEMBER (DRAWING J-3)

A near-longitudinal tube joining the upper parts of the front and main roll bars.

2.8 TRANSVERSAL MEMBER (DRAWING J-4)

A transversal tube joining the lateral roll bars or backstays.

2.9 DIAGONAL MEMBER (DRAWINGS J-5 TO J-9)

A tube between a top corner of the main roll bar, or (in the case of a lateral roll bar) one end of the transversal member, and the lower mounting point of the roll bar on the opposite side or between the upper end of a backstay and the lower mounting point of the other backstay.

2.10 REMOVABLE MEMBER

A member of a safety cage structure which can be removed by unbolting.

2.11 CAGE REINFORCEMENT

A member added to the safety cage structure to improve its strength.

2.12 MOUNTING FOOT

A plate welded to the end of a roll bar tube or member to enable it to be bolted and/or welded to the bodyshell/chassis.

2.13 REINFORCEMENT PLATE

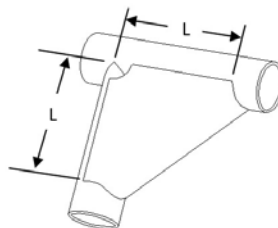
A plate attached to the bodyshell/chassis to support a mounting foot.

2.14 BACKSTAY (DRAWING J-3 AND J-4)

A straight and near-longitudinal member located rearward between the top of the main or lateral roll bar, and the bodyshell/chassis.

2.15 GUSSET (DRAWING J-1)

A reinforcement, made from sheet steel of not less than 1.0mm thick, formed to a U-shape and welded to a bend or junction. The dimension (L) shall be between two and four times the diameter of the larger of the tubes joined.



Drawing J-1

3. APPLICATION

A safety cage structure complying with Schedule J is required as follows:

3.1 RACES (INCLUDES PRACTICE, QUALIFYING AND WARM-UP):

- (a) **Contemporary:** Each vehicle other than a closed, road-registered vehicle participating in a Club or Multi-Club level race.
- (b) **Historic (5th Category):** Each vehicle in a race exclusively for the 5th Category shall be fitted with a safety cage structure as required by the 5th Category Historic Technical Regulations and, where applicable, these regulations.

3.2 SPEED AND REGULARITY EVENTS:

- (a) Each 1st Category vehicle in a Speed or Regularity Event.
- (b) State Championship and above level Speed Event and Regularity Event, other than:
 - (i) a vehicle of the 5th Category (unless otherwise specified in the Historic Technical Regulations); or
 - (ii) a vehicle which is road registered.

3.3 OFF ROAD EVENTS:

- (a) Each Performance 2WD, Extreme 2WD, Production 4WD, Extreme 4WD and Side by Side (SXS) vehicle in an Off Road event.
- (b) Each Extreme 2WD and Extreme 4WD may alternatively comply with the prescriptions of GR9 of Off Road Vehicles – General Requirements.
- (c) Each SXS vehicle shall be approved by Motorsport Australia based on the criteria for a Type 3 safety cage in this schedule.
- (d) Each buggy - Pro, Prolite, Sportslite, Super 1650, Sportsman and Clubman class vehicle shall comply with the prescriptions of GR9 of Off Road Vehicles – General Requirements.

3.4 RALLY/ROAD EVENTS:

- (a) Special Stage Rallies: Each vehicle, other than:
 - (i) for a closed automobile in a Club or Multi-Club Stage 1 Rallysprint; or
 - (ii) for a 5th Category Historic Car entered in a rally, unless the vehicle is required to be fitted with a safety cage structure when entered in a 5th Category, Historic-only circuit race. The safety cage structure fitted shall be to at least the same specification as required in a race. This provision shall apply only to a vehicle subject of and which remains in conformity with, a 5th Category Certificate of Description. This exception does not apply to Historic Touring Cars (Group N) or Historic Production Sports Cars (Group S).
- (b) Speed Limited Special Stage Rallies (as per relevant event regulations): a minimum of a Type 2 safety cage structure incorporating requirements for competition with more than one occupant.



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- (c) Exceptions: A safety cage structure is not mandatory in:
- (i) Road Rallies except where the requirements for Road Rallies as per NRC RFR regulations apply (i.e. Closed Road Special Tests);
 - (ii) a Touring Assembly;
 - (iii) a Navigation Assembly; or
 - (iv) a road event conducted entirely in compliance with the normal civil road regulations. Note: as an example, this exemption does not apply to an event where elements of the competition allow the statutory speed limit to be exceeded.

3.5 NON-SPEED EVENT (I.E. MOTORKHANA, KHANACROSS AND OBSERVED SECTION TRIALS):

As specified under the Auto Test section of the Manual.

3.6 DRIFTING:

As specified in the Drifting Standing Regulations. A safety cage structure is mandatory in each open vehicle.

3.7 INTERNATIONAL EVENTS:

As specified by the FIA. Competitors should note additional or different requirements to Schedule J may apply.

3.8 OTHER EVENTS AND ADDITIONAL REQUIREMENTS:

As specified in the Supplementary Regulations.

4. SAFETY CAGE STRUCTURES BUILT TO PREVIOUS AND FOREIGN REGULATIONS

4.1 AUTOMOBILES BUILT TO PREVIOUS MOTORSPORT AUSTRALIA REGULATIONS:

Where a safety cage structure is fitted to a vehicle which is subject of a Motorsport Australia log book, or where the safety cage structure has been registered or certified by Motorsport Australia, such safety cage structure may continue to be used in competition provided it remains in conformity with the regulations that were in force at the date of issue of either the initial log book for the vehicle, or the registration/certification document.

Regardless of when the car was first subject of a log book, the following requirements apply

- (a) a Type 3 Full Safety Cage structure is the minimum requirement in a closed vehicle in a national or state level race, except in competition exclusively for the 5th Category (and unless otherwise specified in the relevant Historic Technical Regulations);
- (b) a Type 3 Full Safety Cage structure is the minimum requirement in a closed vehicle in a national level rally and/or a Tarmac Rally;
- (c) a Type 3 Full Safety Cage structure with a roof reinforcement configuration complying with Drawing J-14, J-15 or J-16 is the minimum requirement in an open vehicle in a timed rally/trial or off road event;
- (d) protective padding is required in accordance with Article 11.
- (e) It is permitted to add non-compulsory bars to a safety cage structure.
 - (i) This shall not affect the original registration of the structure.
 - (ii) Where a non-compulsory bar is added it shall be attached as specified in these regulations. This shall include material specifications as listed (Table J-1 and J-2).
 - (iii) Any addition to a registered safety cage shall be submitted to Motorsport Australia, with the vehicle logbook for approval and update.
 - (iv) For a certified safety cage, an addendum must be submitted to Motorsport Australia for approval prior to fitment of any non-compulsory member.



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4.2 FOREIGN AUTOMOBILES NOT COMPLYING WITH SCHEDULE J:

- (a) A safety cage structure approved by an ASN other than Motorsport Australia is not necessarily eligible for competition in Australia. Prior to importing a vehicle, a competitor is strongly advised to contact the Motorsport Australia Technical Department for guidance.
- (b) Motorsport Australia may accept a vehicle recognised by Motorsport New Zealand (MSNZ) fitted with a safety cage structure not complying with Schedule J. The safety cage must comply with Schedule J in all respects except for:
 - (i) the main roll bar tubing, the minimum size of which is 38.1mm x 2.5mm; and/or
 - (ii) the use of ERW tubing to MSNZ specification.

In each case:

- (i) the vehicle must be subject of a MSNZ log book;
- (ii) the safety cage structure must be subject of a MSNZ safety cage structure homologation document;
- (iii) the vehicle must have been used in a rally or race competition in New Zealand, evidenced by a valid entry in the log book; and
- (iv) where the dimension of the main roll bar tubing is less than 44.45mm x 2.5mm, the safety cage structure shall incorporate a double diagonal (cross) configuration in the main roll bar in accordance with Drawing J-7.

NOTE:

A vehicle with a 38.1mm diameter main roll bar shall be acceptable only if the MSNZ log book was issued prior to 1 January 2011. This aligns with MSNZ material requirements.

The documents detailed above shall be presented at scrutiny and/or upon request of the Chief Scrutineer.

A vehicle which is normally domiciled and/or is road registered in New Zealand is permitted to compete in rallies in Australia for a maximum period of three calendar months without being required to comply with (iv) of this article, provided that the vehicle complies with MSNZ safety cage regulations. Thereafter the vehicle must either comply fully with the requirements of Schedule J or be exported from Australia before any further dispensation can be considered.

4.3 SAFETY CAGE KITS:

- (a) Despite some manufacturers' claims, not every safety cage structure built from a commercially available 'kit' complies with the current FIA and/or Motorsport Australia regulations.
- (b) A competitor proposing to install such a kit is strongly advised to contact the Motorsport Australia Technical Department for guidance prior to purchasing.

5. REGISTRATION

- (a) A registered safety cage structure is one which has been accepted by Motorsport Australia for a specific vehicle on the basis of a declaration by the manufacturer that it complies with the requirements of this Schedule. This registration shall record the specification of the safety cage structure. The registration number shall be recorded in the log book.
- (b) A safety cage structure fitted to a vehicle for which a log book is issued after 31 December 2000 shall be registered with Motorsport Australia and shall have affixed a valid Motorsport Australia safety cage registration label. Prior to this date, registration is not required.
- (c) Registration is not required when the type and/or status of a competition does not require the use of a safety cage structure.
- (d) Registration of a safety cage structure is not evidence of its compliance with Schedule J.
- (e) A safety cage registration amendment shall be submitted to Motorsport Australia with the vehicle logbook to record an update from the original structure as detailed in Article 4.1(e).

6. CERTIFICATION BY MOTORSPORT AUSTRALIA

- (a) A certified safety cage structure is one which has been approved by Motorsport Australia for a specific vehicle, notwithstanding that it may not comply in all respects with the specifications of this Schedule.

IMPORTANT NOTE:

The certification process does not provide for 'free design' safety cage structures. It is intended primarily to prove that the manufacturer has met the strength criteria through engineering calculation despite the use of material to alternate specifications. A safety cage structure must still comply with the design criteria detailed in the applicable regulations to be eligible for certification by Motorsport Australia.

- (b) Where the vehicle is subject of safety cage certificate, the safety cage certification number shall be recorded in the log book.
- (c) A manufacturer may apply to Motorsport Australia for the certification of a safety cage structure. The certification regulations are available from the Motorsport Australia Technical Department.
- (d) Approval in principle for any proposed design shall be sought from Motorsport Australia prior to commencement of construction.
- (e) A safety cage structure that is homologated/certified after 1996 shall be identified by means of an identification plate affixed to it by the manufacturer. This identification plate must not be copied or moved and shall be embedded or engraved on a metallic plate. The identification plate shall bear the name of the manufacturer, the manufacturer's unique serial number of the safety cage structure and the certification number of the Motorsport Australia certificate (or, where applicable, that of another ASN).
- (f) Where a safety cage structure is subject of a certificate or homologation VO, this documentation shall be presented at event scrutiny and be available to the Chief Scrutineer and/or Technical Commissioner upon request. The safety cage structure must comply with this document at all times.

7. NON-COMPLYING SAFETY CAGE STRUCTURES

Where the type and/or status of the event does not require a vehicle to be fitted with a safety cage structure, the use of a non-compliant structure is permitted, provided it is not deemed unsafe by the Chief Scrutineer.

8. MATERIAL SPECIFICATIONS

8.1 TUBE PROFILE:

Only tubes with a circular section are permitted.

8.2 SPORTS AND TOURING CARS:

The following shall apply to a sports or touring car:

Table J-1

Material	Minimum tensile strength	Minimum dimensions	Use
Cold drawn seamless (CDS) or Cold drawn welded (CDW) unalloyed carbon steel	350 N/mm ²	Metric: 44.45mm x 2.5mm or 50 x 2.0mm	Main roll bar and lateral roll bars
		Imperial: 1.75" x 0.095" or 2.0" x 0.083"	
		Metric: 38mm x 2.5mm or 40mm x 2.0mm	Lateral half roll bars and other parts of the safety cage
		Imperial: 1.5" x 0.095" or 1.6" x 0.083"	

8.3 SINGLE-SEAT AUTOMOBILES:

The following shall apply to a single-seat vehicle weighing 700kg or less, as defined in Article 14. For a vehicle that weighs over 700kg, the material specifications detailed in Article 8.2 shall apply.

Table J-2

Material	Minimum tensile strength	Minimum dimensions	Use
Cold drawn seamless (CDS) or cold drawn welded (CDW) unalloyed carbon steel	350 N/mm ²	Metric: 38mm x 2.5mm or 40mm x 2.0mm	Main roll bar and single brace
		Imperial: 1.5" x 0.095" or 1.6" x 0.083"	
		Metric: 31.7mm x 2.5mm or Imperial: 1.25" x 0.095"	Dual braces



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44.45mm	1" 3/4	2.6mm	0.102"	7330mm	24ft

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8.4 STEEL TYPE AND COMPOSITION:

The steel shall be cold drawn seamless (CDS) or cold drawn welded (CDW) unalloyed carbon steel containing a maximum of 0.3% of carbon, with adequate elongation and weldability properties. For an unalloyed steel, the maximum content of additives is 1.7% for manganese and 0.6% total of all other elements.

8.5 MATERIAL NOT IN COMPLIANCE:

Motorsport Australia may accept other steels and sizes not in compliance with this article through the process of safety cage structure certification.

9. BENDING

The tubing shall be bent by a cold working process and the centreline bend radius shall be at least three times the tube diameter.

If the tubing is ovalised or otherwise distorted during bending, the ratio of minor to major diameter shall be 0.9 or greater. The surface of the tube at the bend shall be smooth and even, without ripples or cracks.

10. GUIDANCE ON WELDING

- (a) Welding shall be carried out along the whole perimeter of each tube joint and shall be of the highest possible quality with full penetration, preferably using a gas-shielded arc.
- (b) Although good external appearance of a weld does not necessarily guarantee its quality, a weld of poor appearance may indicate that it is unsatisfactory.
- (c) When using heat-treated steel any special instructions of the tube manufacturer shall be followed (e.g. ambient temperature, special electrodes, gas protected welding).

11. PROTECTIVE PADDING

11.1 HELMET AND HEAD PROTECTION:

- (a) For each automobile subject of a safety cage structure where the helmet of an occupant could come into contact with the safety cage structure and/or any safety cage bar that is directly above or to the side of the helmet of an occupant, protective padding shall be fitted to that area, which complies with:
 - (i) International Competition and below, FIA standard 8857-2001, type A (see FIA Technical List No 23 Roll Cage Padding Homologated by the FIA); or
 - (ii) For National Competitions and lower, SFI specification 45.1. For each 1st Category vehicle, the use of SFI specification 45.2 for the head rest is permitted.
- (b) Where it can be demonstrated that the fitment of protective padding unduly hampers the occupants' ability to operate the vehicle in a safe manner, the requirement for padding may be negated in the immediate area of concern at the discretion of the Chief Scrutineer.

11.2 BODY PROTECTION:

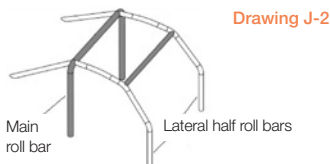
Where the body of an occupant could come into contact with the safety cage, it is recommended that flame retardant padding be fitted. FIA standard 8857-2001 or SFI specification 45.1 padding.

12. SPECIFICATIONS – CLOSED SPORTS AND TOURING CARS (TYPE 3)

12.1 BASIC STRUCTURE:

The basic structure must be made according to one of the following designs:

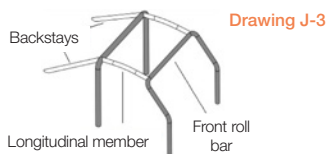
1 main roll bar + 2 lateral half roll bars + 1 transversal member + 2 backstays + 6 mounting feet (Drawing J-2). This structure shall be referred to as 'Type 3 Safety Cage'.



Drawing J-2

OR

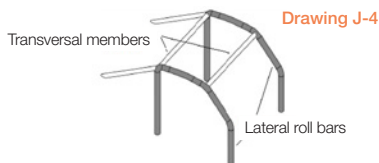
1 main roll bar + 1 front roll bar + 2 longitudinal members + 2 backstays + 6 mounting feet (Drawing J-3)



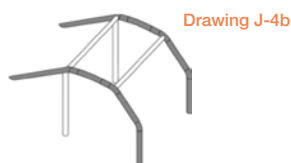
Drawing J-3

OR

2 lateral roll bars + 2 transversal members + 2 backstays + 6 mounting feet (Drawing J-4).



Drawing J-4



Drawing J-4b

(a) **Main Roll Bar**

The vertical parts of the main roll bar must be as close as possible to the interior contour of the bodyshell and may have only one bend in each lower near-vertical part.

(b) **Front Roll Bar/Lateral Roll Bar**

- (i) Only one bend is permitted in the front leg of a front roll bar or of a lateral roll bar.
- (ii) The front leg must follow the windscreen pillar as closely as possible with the sole bend in its lower near-vertical part.
- (iii) **For non-international competition**, the front leg may depart from the contour of the windscreen ('A') pillar to enable it to be located rearward of the dashboard.
- (iv) The sole bend permitted in the front leg may be incorporated in its upper part, to enable it to follow the windscreen pillar.
- (v) Each connection between a lateral half roll bar and the main roll bar (J-2), between a longitudinal member and a front or main roll bar (J-3), and between a transversal member and a lateral roll bar (J-4), shall be situated at the roof level.

There shall be not more than four removable connections in the basic structure at the roof level.

Note: A removable connection fitted at the top of a backstay is not considered to be at roof level.

(c) **Backstay:**

- (i) Each backstay:
 - (A) shall be attached to the main or lateral roll bar near the roofline;
 - (B) be positioned no further than 100mm from the top outer bend on each side of the vehicle;
 - (C) shall not be below the connection of the main roll bar and the front lateral or half lateral roll bar.
 - (D) must form an angle between 30° and 60° from the vertical; and
 - (E) must run rearwards and be straight and be as close as possible to the interior side panels of the bodyshell.
- (ii) A removable connection may be used.
- (iii) For closed sports cars and utilities, and where permitted by the relevant Group technical regulations, the backstays may extend beyond the interior/cockpit of the vehicle in order to achieve an angle between 30° and 60° with the vertical. If the required angle cannot be achieved, certification by Motorsport Australia is required.

12.2 DESIGN:

In addition to the basic structure, the safety cage must be completed by the addition of compulsory members and reinforcements to which optional members and reinforcements may be added (see Article 15).

12.3 COMPULSORY MEMBERS AND REINFORCEMENTS:

(a) **Diagonal Member:**

The safety cage structure designs detailed in Drawings J-5, J-6, J-7, J-8 and J-9 shall be referred to as 'Type 3 Full Safety Cage'.

- (i) **For a competition with a driver only**, the safety cage structure shall be configured in accordance with Drawing J-5, J-6, J-7 or J-8, incorporating at least one continuous diagonal member. If configured in accordance with Drawing J-5 or J6, the diagonal shall have the upper end attached on the driver's side.

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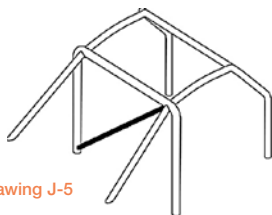
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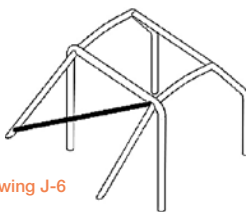
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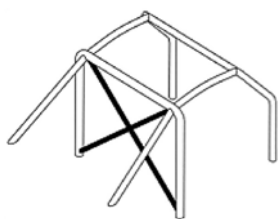
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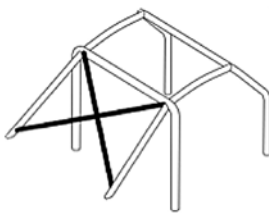
Drawing J-5



Drawing J-6



Drawing J-7



Drawing J-8



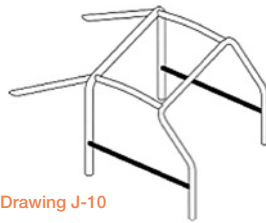
Drawing J-9

- (ii) Each diagonal member may be removable and must be a straight and single continuous piece.
- (iii) For Drawing J-5 the upper end of the diagonal member shall join the main roll bar no further than 100mm from its junction with the backstay. For Drawing J-6 the upper end of the diagonal member shall join the backstay no more than 100mm from its junction with the main roll bar. See Drawing J-62 for the measurement.
- (iv) The lower end of the diagonal shall join the main roll bar or the backstay no further than 100mm from the mounting foot.
- (v) **For a competition with more than one occupant**, the safety cage structure shall be configured in accordance with Drawing J-7 incorporating 'double diagonal' members in the main roll bar.
- (vi) A 'double diagonal' configuration detailed in Drawing J-7, J-8 or J-9 is recommended in which case:
 - (A) One of the 'double diagonal' members shall be a single continuous piece and shall be straight.
 - (B) The intersection of the diagonal members must be reinforced by at least two gussets in accordance with Article 2.15.
 - (C) The diagonal members may be removable.
 - (D) The lower end of each diagonal shall join the main roll bar or backstays no further than 100mm from the mounting foot (see Drawing J-62 for the measurement).
- (vii) For Drawing J-7 the upper end of each diagonal member shall join the main roll bar no further than 100mm from its junction with the backstay.
- (viii) For Drawing J-8 the upper end of each diagonal member shall join the backstay no more than 100mm from its junction with the main roll bar. See Drawing J-62 for the measurement.
- (ix) In addition to the minimum configuration detailed in Drawing J-7 or J-8, the combination of diagonal members (J7 + J6 or J8 + J5 or J9) may also be used.

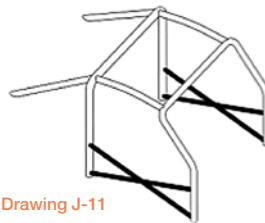
(b) **Door bars:**

For Off Road competitions, State Championships and above level Race and Rally competitions (and recommended in other competitions):

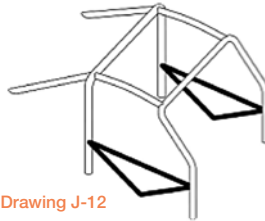
- (i) One or more door bars must be fitted at each side of the vehicle according to Drawings J-10, J-11, J-12 or J-13.
- (ii) For State Rally Championships and above, it is mandatory that 2 or more door bars shall be fitted at each side of the vehicle in accordance with Drawings J-11, J-12 or J-13.
- (iii) Door bars may be removable, subject to the use of dismantable joints complying with Article 16.
- (iv) The side protection shall be as high as possible, subject to its upper attachment point not being higher than half the height of the door opening measured from its base.
- (v) If these upper attachment points are located in front of or behind the door opening, this height limitation is for the intersection of the door bar and the door opening. Refer Drawing J-59.
- (vi) In the case of door bars in the form of an "X" (Drawing J-11) it is mandatory that this joint be reinforced by two gussets in accordance with Article 2.15 (see Drawing J-59). It is recommended that the "X" be formed by two continuous bars which are joined in the centre, in which case a gusset reinforcement must be fitted.
- (vii) In accordance with the requirements for a windscreen pillar reinforcement the connection of the windscreen pillar reinforcement (Drawing J-17) to the door bar/s is permitted.



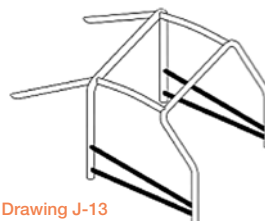
Drawing J-10



Drawing J-11



Drawing J-12



Drawing J-13

(c) **Roof Reinforcement:**

For each Off Road, National Race and State and above level Rally competition, (and recommended in other competitions).

- (i) The upper part of the safety cage structure shall comply with Drawing J-14, J-15 or J-16. However, for a driver only competition, in the case of Drawing J-14, only one diagonal member is required subject to its front connection being on the driver's side.
- (ii) The reinforcements may follow the curve of the roof.
- (iii) The ends of the reinforcements must be less than 100mm from the junction between roll bars and members (not applicable to the top of the 'V' formed by reinforcements in Drawings J-15 and J-16).
- (iv) The configuration of J-16 shall only be used in conjunction with J-34.
- (v) Where the roof reinforcement is in the form of an "X" (Drawing J-14) it is mandatory that this joint be reinforced by two gussets in accordance with Article 2.15.

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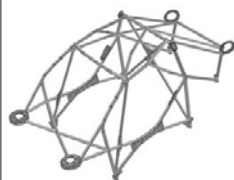
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Drawing J-14



Drawing J-15

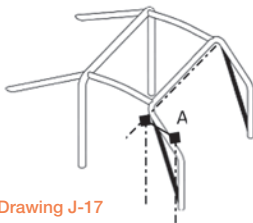


Drawing J-16

(d) **Windscreen Pillar Reinforcement:**

For each Off Road, National Race and State and above level Rally competition (and recommended in other competitions).

- (i) A windscreen pillar reinforcement member shall be fitted on each side of the front roll bar if dimension "A" is greater than 200mm (refer Drawing J-17).



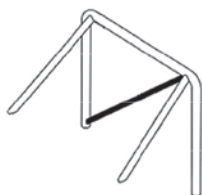
Drawing J-17

- (ii) The windscreen pillar reinforcement member may be bent on condition that it is straight in side view and that the angle of the bend does not exceed 20°.
- (iii) Its upper end shall be less than 100mm from the junction between the front (lateral) roll bar and the longitudinal (transversal) member (refer Drawing J-62)
- (iv) The lower end shall connect to the front leg and shall be within 100mm of the front mounting foot (refer Drawing J-62).
- (v) If this reinforcement intersects the door bars, it must be split in several parts. The intersection of these joints must be reinforced by at least two gussets in accordance with Article 2.15.

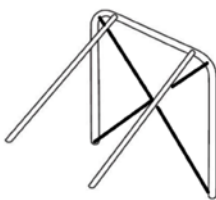
13. SPECIFICATIONS – OPEN AND CLOSED CARS (TYPE 2)

13.1 RACE (OPEN SPORTS CARS), RALLY (SPEED LIMITED), SPEED (STATE LEVEL AND ABOVE) AND DRIFTING:

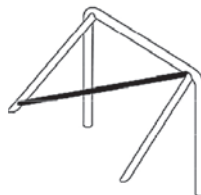
- (a) Basic Structure: The basic structure shall be constructed according to the following design:
1 main roll bar + 2 backstays + 1 diagonal + 4 mounting feet (refer Drawing J-18 or J-19) this structure shall be referred to as 'Type 2 Half Safety Cage'.



Drawing J-18



Drawing J-18 Dual Occupant

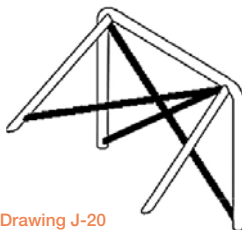


Drawing J-19

(b) **General:**

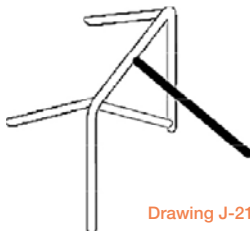
- (i) For each vehicle derived from a registrable production vehicle, the top of the main roll bar tubing shall be a minimum of 50mm above the top of the driver's helmet when the driver is in the normal seated position.
- (ii) Each vehicle which is not derived from a registrable production vehicle shall have two rollover structures and shall comply with the following:
- (A) The principal structure shall be positioned behind the driver.
- (B) The secondary structure shall be in front of the steering wheel, but no more than 250mm forward of the top of the steering wheel rim in any position.
- (C) Under no circumstance may the driver's helmet be less than 50mm below a line drawn between the highest points of the principal and secondary structures (refer Drawing J-26).
- (D) In conjunction with the vehicle's structure the safety cage structure shall not leave unprotected any part of the driver's shoulders when viewed from front or rear.

- (c) **Stays:**
- (i) For open cars, the stays may be fitted either forward or backward of the main roll bar.
 - (ii) For closed cars, the stays shall only be fitted backward of the main roll bar. Each stay shall be straight and shall form an angle of between 30° and 60° from the vertical.
- (d) **Diagonal Member:**
- (i) The safety cage structure shall have a diagonal member configured as defined in Drawing J-18 or J-19 with the upper end of the diagonal attached on the driver's side.
 - (ii) For a competition with more than one occupant, the safety cage structure shall be configured in accordance with Drawing J-18 Dual Occupants incorporating 'double diagonal' members in the main roll bar.
 - (iii) Each diagonal member shall be straight and may be removable.
 - (iv) The upper end of the diagonal shall join the main roll bar within 100mm of its junction with the backstay, or the backstay within 100mm of its junction with the main roll bar (refer Drawing J-62 for the measurement).
 - (v) One or more additional diagonal member/s is recommended, and a double diagonal configuration is permitted in the main roll bar and between the stays (refer Drawing J-20).



Drawing J-20

- (e) **Additional Bracing:**
- (i) A forward mounted reinforcement is permitted (refer Drawing J-21).



Drawing J-21

- (A) The lower portion of this reinforcement may be mounted in the front passenger space, in which case a passenger is not permitted.
 - (B) Where fitted, the driver's helmet shall be at least 50mm from the member when seated in the normal driving position.
 - (C) A seat incorporating a lateral head restraint is recommended. Refer also to Article 11.1 regarding protective padding.
- (ii) If door bars are incorporated into a Type 2 Half Safety Cage, the forward end of each door bar member or the reinforcement detailed in Drawing J-21 shall be fitted with a mounting foot which shall be attached to the bodyshell/chassis in accordance with Article 18.
 - (iii) It is recommended that each competitor consider the fitment of additional reinforcing members and/or a Type 3 Full Safety Cage.
 - (iv) For an open car fitted with a Type 3 Full Safety Cage, the basic structure detailed in Article 12.1 is the minimum requirement for the elements of the safety cage structure forward of the main roll bar, regardless of event status unless a higher specification is mandated in the regulations applying to that competition.

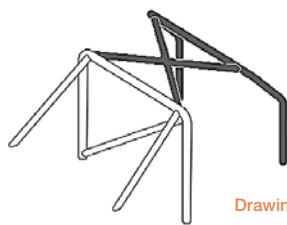
13.2 RALLY AND OFF ROAD:

Each open vehicle used in a Rally/Road or Off Road event must be fitted a Type 3 Full Safety Cage structure which incorporates one of the roof reinforcement options detailed in Drawing J-14, J-15 or J-16, regardless of event status, except as detailed in Article 3.4(c).

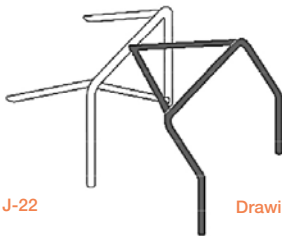
13.3 TARGA ROOF AUTOMOBILES (NOT APPLICABLE FOR RALLY/ROAD OR OFF ROAD EVENTS):

A targa roof is one where the top of the front windscreen support is connected to the rear window ('B') pillar, if fitted, but only in a narrow section inboard of the tops of the windscreen (or 'A') pillars or where there is no permanent connection at roof level between the A and B pillars.

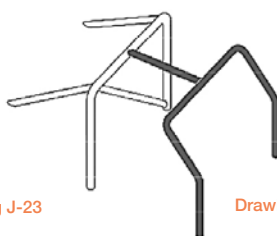
A vehicle deemed to have a targa roof may compete in a race, speed, modern regularity or drift event with a safety cage structure complying with Drawing J-22, J-23 or J-24 and with Article 13.1(a).



Drawing J-22



Drawing J-23



Drawing J-24

14. SPECIFICATIONS – SINGLE-SEAT CARS (TYPE 1)

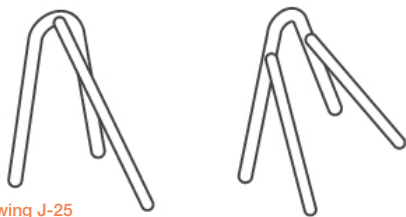
Except where varied by this regulation or Group or Technical regulations approved by Motorsport Australia, a purpose designed single seat vehicle shall comply with this Article. This Article shall not apply to a vehicle in which the passenger seat has been removed nor to any vehicle in Off Road competition.

14.1 OPEN COCKPIT CARS:

Each vehicle shall be fitted with a principal structure and a secondary structure.

(a) Principal Structure:

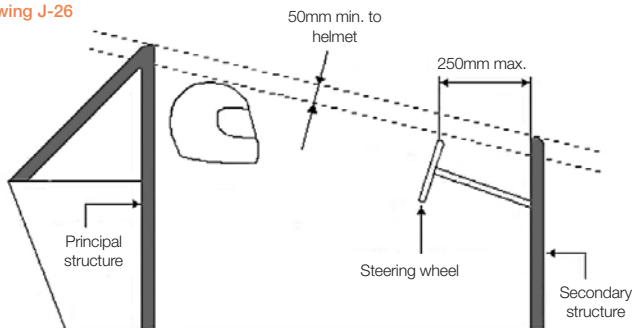
- (i) The principal structure shall consist of a main roll bar and a backstay as a minimum (refer Drawing J-25). This structure shall be referred to as 'Type 1 Solo Roll Bar'.



Drawing J-25

- (ii) The principal structure must be positioned behind the driver.
- (iii) The main roll bar shall be made in one piece, without joints.
- (iv) There shall be at least one brace from the top of the main roll bar.
- (v) The brace/s shall form an angle between 30° and 60° with the vertical.
- (vi) The diameter and material of the brace/s shall comply with Article 8.3.
- (vii) Where two braces are used, they may be fitted both sides of the roll bar and shall be within 100mm of the top of the main roll bar.
- (viii) The stays may face rearward and/or forward.
- (ix) The main roll bar (together with any additional members and/or a head rest) shall be configured such that the driver's helmet cannot pass through it.
- (x) The bend radius on the centreline of the main roll bar shall be between two and four times the tube diameter.
- (xi) In conjunction with the vehicle's structure, the main roll bar shall not leave unprotected any part of the driver's shoulders, when viewed from the front or rear.
- (xii) Each leg of the main roll bar shall be straight from the uppermost bend to within 100mm of where it meets the chassis.
- (xiii) Any removable connection shall comply with Drawings J-47 to J-56.
- (xiv) Under no circumstance may the driver's helmet be less than 50mm below a line drawn between the highest points of the principal and secondary structures (refer Drawing J-26).

Drawing J-26



(b) **Secondary Structure:**

A substantial secondary structure shall be incorporated forward of the steering wheel, but no more than 250mm forward of the top of the steering wheel rim.

14.2 CLOSED CARS:

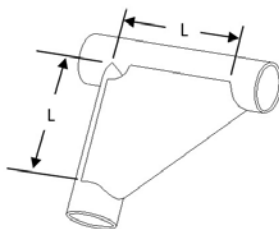
Each vehicle shall be fitted with a safety cage structure complying as a minimum with Drawing J-5, J-6, J-7 or J-8 and Article 12. Refer also to Compulsory Members and Reinforcements.

14.3 ALTERNATIVE DESIGNS:

A safety cage structure not in compliance with Schedule J specifications may be approved through the Motorsport Australia certification process. The static load tests detailed in by the FIA shall apply. Approval in principle for any proposed design shall be sought from Motorsport Australia prior to commencement of construction.

15. REINFORCEMENT OF BENDS, JUNCTIONS AND TUBES-GUSSETS

- (a) A minimum of two gussets complying with Article 2.15 (and Drawing J-1), for safety cages registered on or after 1 July 2018, shall be fitted onto the following junctions between the:
 - (i) diagonal members of the main roll bar or rear backstay (Type 3 Drawing J-7, J-8, J-9 and Type 2 Drawing J-20);
 - (ii) roof reinforcements (Drawing J-14);
 - (iii) door bars (Drawing J-11); and
 - (iv) door bars and the windscreen pillar reinforcement (Drawing J-17).
- (b) If a door bar and the windscreen pillar reinforcement (where fitted) are not in the same plane (intersecting), the reinforcement gusset/s may run horizontally or vertically and be fabricated from sheet metal.
- (c) Any hole in a tube forming part of a safety cage structure shall be fitted with a bush, the wall thickness of which is at least the same as that of the tube. The bush shall be welded around its whole perimeter at both exit points.



Drawing J-1

15.1 OPTIONAL MEMBERS AND REINFORCEMENTS:

(a) **General:**

Except where otherwise specified in Articles 12, 13 and 14 members and reinforcements shown in Drawings J-14 to J-16, J-29 to J-33 and J-35 to J-45 are optional.

- (i) Installation shall be by welding or by dismantable joints detailed in these regulations.
- (ii) Members and reinforcements may be used separately or in combination.

(b) **Backstay Diagonals** (Drawing J-33):

The configuration of Drawing J-33 may be replaced with that of Drawing J-34 only if a roof reinforcement complying with Drawing J-16 is used.

(c) **Front Suspension Mounting Points** (Drawing J-37):

Where fitted, they shall be mounted to the front suspension top mounting points.

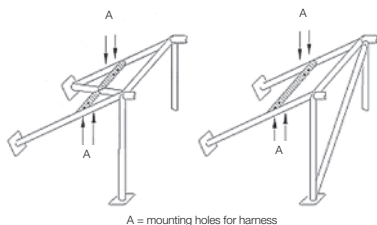
(d) **Transversal Members** (Drawings J-38 to J-42):

- (i) The transversal member fixed to the front roll bar shown on Drawing J-41 shall not encroach upon the space reserved for the occupants and shall not be positioned below the steering column.
- (ii) The transversal member shall be placed as high as possible subject to its lower edge not being higher than the uppermost point of the dashboard.
- (iii) For members shown on Drawings J-38 and J-39, the angle between the central leg and the vertical shall be at least 30°.

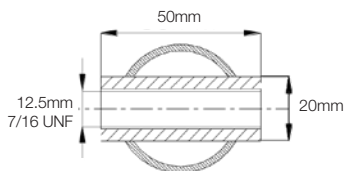
(e) **Safety Harness Mounting to a Transversal Member:**

- (i) Welded transversal members fitted on the main roll bar or between the backstays may be used for the safety harness mountings provided the installation complies with Schedule I.
- (ii) It is highly recommended that the requirements for FHR are observed for positioning of mountings for shoulder straps.
- (iii) The harness shall be installed in accordance with the manufacturer's instructions and Schedule I.
- (iv) Any bolted attachment shall comply with Drawing J-28. Where a bush is inserted (which may be threaded) the bush shall be welded around its whole perimeter at both exit points.
- (v) The harness/es shall be mounted using bolts of M12 8.8 (ISO standard) or 7/16UNF or to manufacturer's specification.

Drawing J-27



Drawing J-28



15.2 REINFORCEMENT OF BENDS AND JUNCTIONS:

Reinforcements shall consist of tubes or gussets made from sheet metal formed into a U-shape complying with Article 2.15. The end of a tubular reinforcement shown in Drawings J-43, J-44 and J-46 shall not be attached beyond the mid-point of the member.

16. REMOVABLE MEMBERS

- Removable members may be incorporated subject to the use of dismantable joints complying with Drawings J-47 to J-57 or a joint Homologated by the FIA or Motorsport Australia or other ASN with the approval of Motorsport Australia. Such joints shall not be welded once assembled.
- Any fastener used shall have a minimum quality of 8.8 (ISO standard).
- Dismountable joints complying with Drawings J-47, J-50, J-53, J-56 and J-57 shall be used only for attaching removable members and reinforcements described by Article 12.3. They are not permitted for joining the upper parts of the main roll bar, the front roll bar, the lateral half roll bars or the lateral roll bars (refer Drawings J-2, J-3 and J-4 or a joint Homologated by the FIA or Motorsport Australia or other ASN with the approval of Motorsport Australia).

17. ADDITIONAL CONSTRAINTS

- The safety cage structure shall be entirely contained longitudinally between centrelines of the front and rear axles. For international competition, refer FIA regulations.
- Supplementary reinforcements exceeding these limits are authorised between the safety cage structure and the anchorage points of the rear anti-roll bars on the bodyshell/chassis. Each of these anchorage points may be connected to the safety cage structure by a single tube with maximum dimensions of 30mm x 1.5mm.
- In frontal projection, no additional member or reinforcement of the front roll bar (including the front legs) shall be visible beyond the shaded area of the windscreen, as shown in Drawing J-58.
- In side projection, any reinforcements in the front door aperture shall comply with the following criteria (refer Drawing J-59):
 - Dimension A shall be a minimum of 300mm
 - Dimension B shall be a maximum of 250mm
 - Dimension C shall be a maximum of 300mm
 - Dimension E shall not be more than half the height of the door aperture (H).

18. MOUNTING OF THE SAFETY CAGE STRUCTURE TO THE BODYSHELL/CHASSIS

18.1 MINIMUM MOUNTING POINTS:

The minimum mounting points are:

- one for each pillar of the front roll bar;
- one for each pillar of the lateral roll bars or lateral half roll bars;
- one for each pillar of the main roll bar;
- one for each backstay.

18.2 MOUNTING POINTS OF THE FRONT, MAIN AND LATERAL ROLL BARS OR LATERAL HALF ROLL BARS:

- Each mounting point shall include a mounting foot consisting of a plate at least 3mm thick.
- Where attached by bolts, each mounting foot shall be attached by at least three bolts to a steel reinforcement plate at least 3mm thick and of at least 120cm² area which is welded to the bodyshell.
- Where attached by bolts, the angle between any two bolts shall not be less than 60° (measured from the tube axis at the level of the mounting foot circumference – Drawing J-60).
- The area of 120cm² of the reinforcement plate shall be the contact surface between the reinforcement plate and the bodyshell. Refer Drawings J-60 to J-66. For Drawing J-62 the reinforcement plate need not be welded to the bodyshell.
- Fasteners shall have a minimum diameter of M8 and a minimum quality of 8.8 (ISO standard) and shall be self-locking or fitted with lock washers.

(f) Mounting Points - non-international use only

As an alternative to welding reinforcement plates to the chassis of the vehicle detailed in Article 18.2 and 18.3, each attachment point of the safety cage structure may be fitted with a mounting foot and counterplate complying with Table J-3 below (see also Drawing J-69).

The mounting foot, complying with the area requirements shown below may be welded to the bodyshell or be a fully welded mount structure (box) to the bodyshell, in which case the use of bolts and the counterplate is not required (see Drawing J-70).

Table J-3

Mounting Foot Application	Minimum Area	Minimum single dimension
Automobiles under 700kg	65cm ²	55mm
701kg to 1150kg	75cm ²	65mm
Over 1151kg	100cm ²	75mm
Counterplate	45cm ²	Matching upper plate

18.3 MOUNTING POINTS OF THE BACKSTAYS:

- (a) Each backstay shall be secured by at least two M8 (minimum) 8.8 (ISO standard) bolts with mounting feet of an area at least 60cm² (refer Drawing J-67).
- (b) Alternatively it shall be secured by a single M10 8.8 (ISO standard) bolt in double shear (refer Drawing J-68), provided each mounting plate is at least 3mm thickness steel with a bush fully welded into the backstay.

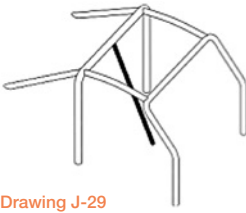
NOTE: Refer also to Article 18.2(c) for alternate provisions for non-international safety cage structures.

18.4 ADDITIONAL FASTENERS AND WELDING:

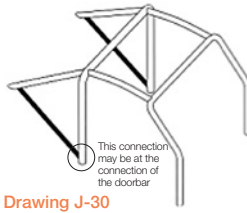
- (a) Additional fasteners and/or fasteners of larger dimensions may be used.
- (b) The safety cage may be attached to the bodyshell/chassis by welding or bolting in locations additional to the minimum mounting points detailed in Article 18.1.

18.5 SPECIAL CASES:

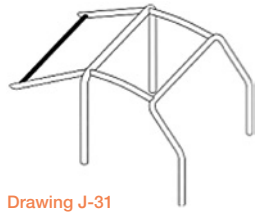
- (a) **Non-steel bodyshells/chassis:**
 - (i) For non-steel bodyshells/chassis, any welding between the safety cage structure and the bodyshell/chassis is prohibited.
 - (ii) In addition to mounting by bolts, only the bonding of the reinforcement plate to the bodyshell/chassis is permitted.
 - (b) **Spaceframe Construction:**
 - (i) The safety cage structure of a spaceframe constructed vehicle shall comply with Schedule J from at least above the predominantly horizontal portion of the chassis.
- Parts of the safety cage structure may extend below this horizontal plane and become integral with the chassis. The safety cage structure shall also comply with any relevant Group or other technical regulations.



Drawing J-29



Drawing J-30



Drawing J-31



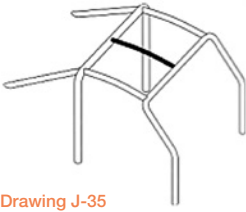
Drawing J-32



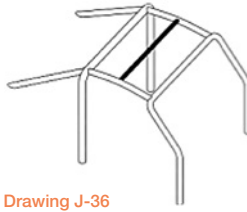
Drawing J-33



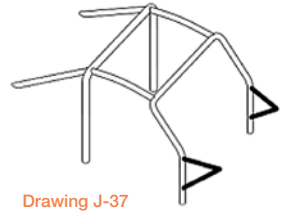
Drawing J-34



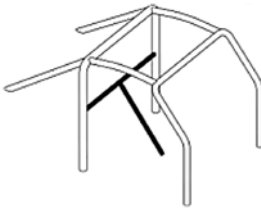
Drawing J-35



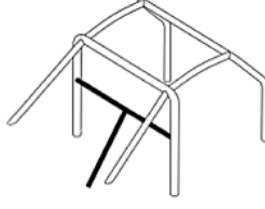
Drawing J-36



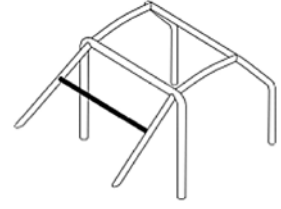
Drawing J-37



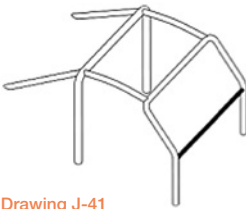
Drawing J-38



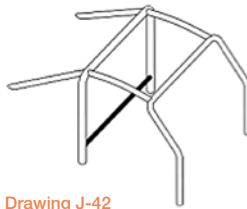
Drawing J-39



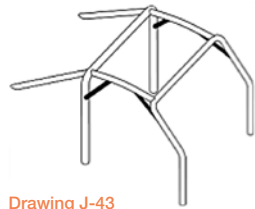
Drawing J-40



Drawing J-41



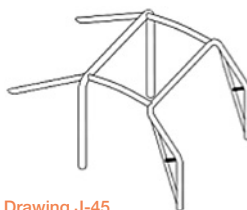
Drawing J-42



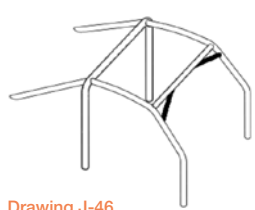
Drawing J-43



Drawing J-44

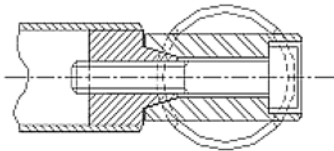
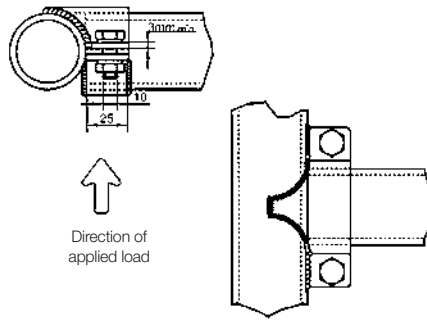


Drawing J-45

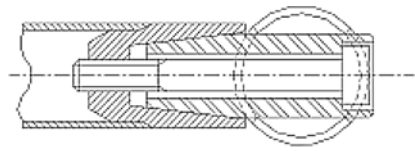


Drawing J-46

Drawing J-47



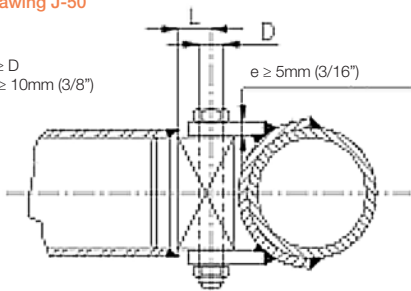
Drawing J-48



Drawing J-49

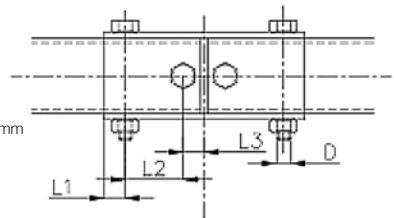
Drawing J-50

$L \geq D$
 $D \geq 10\text{mm}$ (3/8")



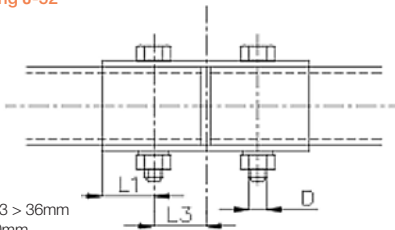
Drawing J-51

$L1 = L3 > 18\text{mm}$
 $L2 \geq 36\text{mm}$
 $D = 8\text{mm}$

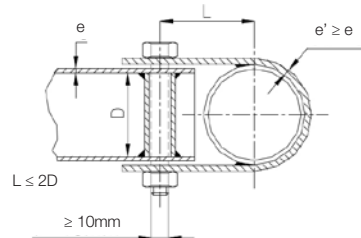


Drawing J-52

$L1 = L3 > 36\text{mm}$
 $D = 10\text{mm}$

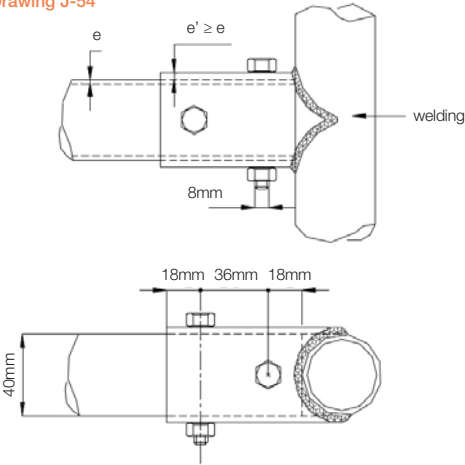


Drawing J-53

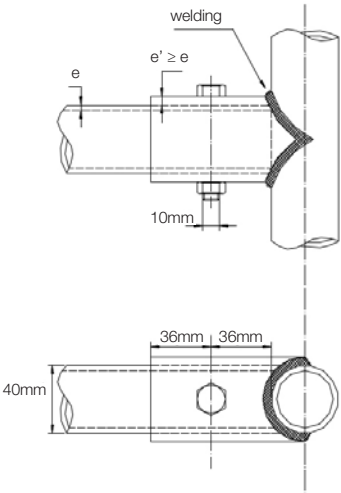


$L \leq 2D$
 $\geq 10\text{mm}$
 L must be minimum
The clamp width must be at least 25mm

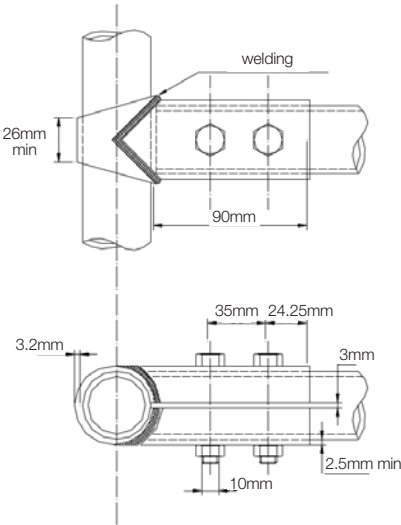
Drawing J-54



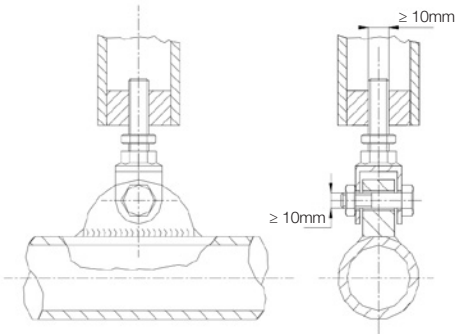
Drawing J-55



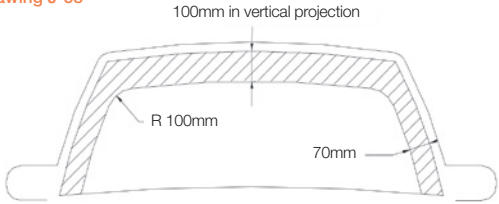
Drawing J-56



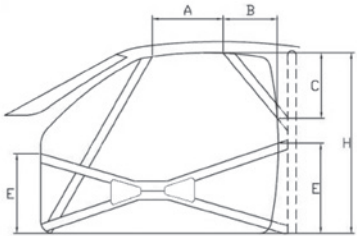
Drawing J-57



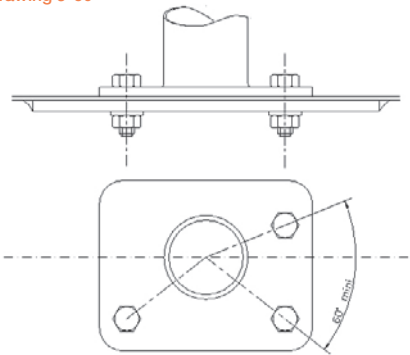
Drawing J-58



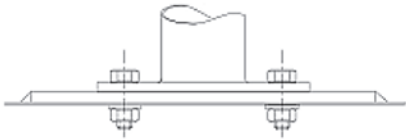
Drawing J-59



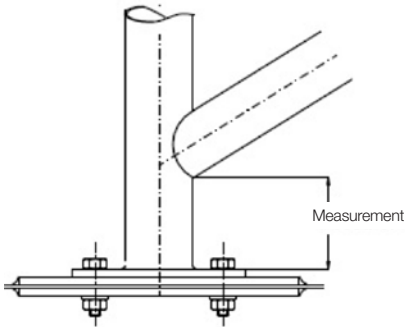
Drawing J-60



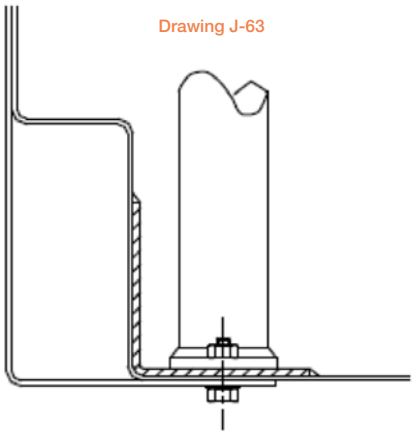
Drawing J-61

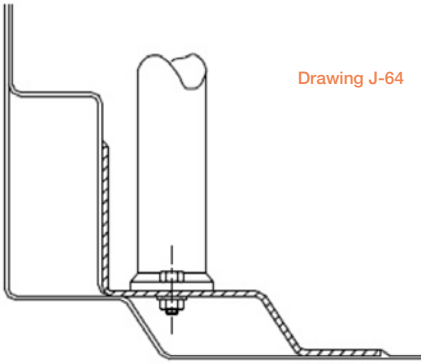


Drawing J-62

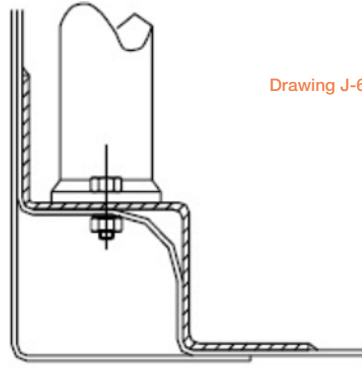


Drawing J-63

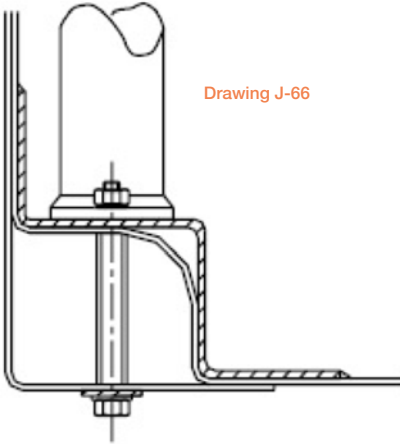




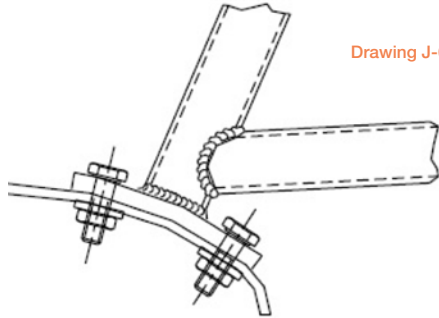
Drawing J-64



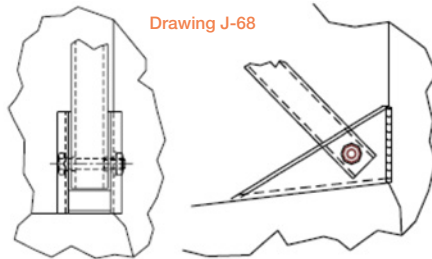
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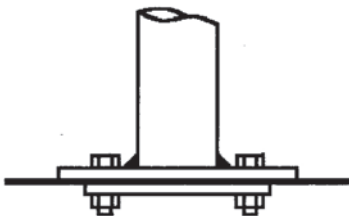
Drawing J-66



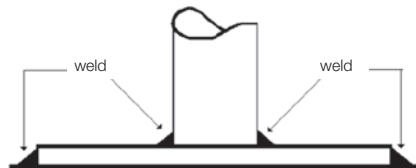
Drawing J-67



Drawing J-68



Drawing J-69



Drawing J-70

SCHEDULE K – MARKINGS ON AUTOMOBILES

1. INTRODUCTION

- (a) For an international competition, refer to the ISC.
- (b) Motorsport Australia retains the right to prohibit any marking on an automobile.
- (c) Markings in accordance with Schedules A, B and C shall be displayed.
- (d) Motorsport Australia may approve regulations which vary this Schedule.

2. MANDATORY IDENTIFICATION

2.1 Competition Numbers

Competition numbers shall be displayed in accordance with this Schedule and be clearly visible to the satisfaction of the Clerk of the Course. The location and size of competition numbers shall be in accordance with the following table:

Discipline	Application	Location	Front number	Side numbers
Racing	1st Category (Racing Automobiles)	Front and Sides	Nose of automobile. Size: Figure K-1	End plate of rear wing (where fitted) or side of body Size: Figure K-1
	2nd Category (Sports Open Automobiles)	Front and Sides	Forward of centreline of front wheels. Size: Figure K-1	Between front and rear wheels Size: Figure K-1
	2nd Category (Sports Closed Automobiles)	Front and Sides	Windscreen for all closed automobiles on the opposite side to the driver. Size: Figure K-2	Between the 'B' pillar and the trailing edge of the front wheelarch. Size: Figure K-1
	3rd Category (Touring Cars)			
	5th Category (Automobiles)	Refer 5th Category General Requirements		
	Superkarts	Refer Superkart Regulations		
	All automobiles	Sides	As required by organisers	Front doors, rear windows or side of cockpit* as appropriate. Size: Figure K-2
Speed Events	All automobiles	Sides. Refer Supplementary Regulations		
Road Events	ARC	Refer Australian Rally Championship Regulations		
	Other Events	Leading edge of front doors Size: Figure K-1 Windscreen, opposite side to the driver Size: Figure K-2 Or as per Supplementary Regulations		
Off Road	Refer Off Road Regulations			
Autotests	Refer Supplementary Regulations			

* **Note:** Defined as a vertical surface alongside the cockpit or the most prominent position possible on the side of the automobile.

3. CIRCUIT EVENTS

- (a) A triple digit competition number is not permitted on a 1st Category or 2nd Category (Open) automobile.
- (b) A competition number in accordance with Figure K-1 shall be black on a white background.
- (c) A competition number in accordance with Figure K-2 shall be "Dayglo" yellow without background.
- (d) A competition number shall be of a style similar to below:

Arial Bold **1234567890**

Note: Styles that comply with the above sample are "Helvetica Bold", "Zurich Bold" and "Arial Bold"

- (e) (e) Competition number "1" is reserved for the use of the current Australian Champion in each category or discipline. Where a champion elects not to use "1", it shall be withheld from use for the appropriate period.
- (f) A State Council may allocate competition number "1" for use by the State Champion in an event wherein the National Champion of that discipline is not competing.
- (g) A competition number commencing with "0" is not permitted
- (h) An automobile driven in a race by a Provisional Competition Licence holder shall display a "P" plate as approved by Australian authorities for civil probationary or provisional drivers. The "P" shall be red on a white background or white on a red background, displayed so it is clearly visible from the rear of the automobile.
- (i) In all races forming part of an Australian Championship for closed automobiles the surname of each driver must be displayed on the lower edge of each rearmost side window or, if this is not possible, in a suitable location as approved by the Chief Scrutineer. The letters shall be of uniform style, 100mm and 60mm, white in colour and without background, using a capital for the first letter of the name, and lower case for all other letters (see figure K-3). For grammatical correctness other letters of the surname may use a capital letter. The typestyle shall be as required for competition numbers (see article 3.4). In all other competitions for closed automobiles, such display is optional, but if used, shall comply with this Regulation.

Figure K-1

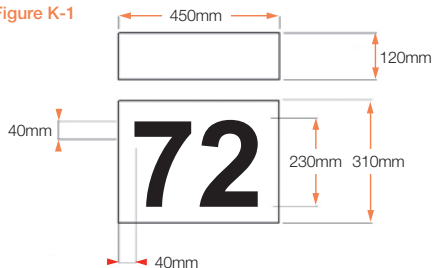


Figure K-2

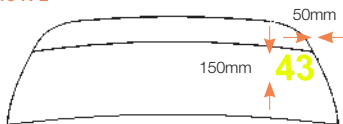


Figure K-3



Figure K-4



Figure K-5



- (i) For international events the national flag of the ASN of each driver and co-driver where applicable shall be displayed adjacent to the name.

4. MOTORSPORT AUSTRALIA LOGO

With the exception of the 5th Category, each automobile in a State championship competition or superior status shall display the Motorsport Australia logo positioned adjacent to each side competition number. Motorsport Australia logo stickers are available from a State office (Figure K-5).

5. MANDATORY ADVERTISING SPACE

The space above each side competition number shall be reserved for advertising. This space may be used by Motorsport Australia, which may assign its use to the Organiser. If coachwork limitations make such space unavailable an equivalent space shall be reserved in the immediate vicinity of the competition number.

6. CONFLICTING SIGNS

The Clerk of the Course may require the removal of any sign deemed to cause confusion with a competition number

7. OTHER MARKINGS

The badge of a Motorsport Australia-affiliated club, a civil registration label or civil number plate shall not be deemed to be signage.

8. OTHER SIGNAGE

- On the top of the windscreen on a background free as to colour and depth, subject to the lettering and devices being placed only within the upper 200mm of the windscreen surface. Specific Sporting Regulations may vary this requirement.
- On the rear window up to 85mm depth located at the top or bottom of the window provided that it does not interfere with the driver's visibility.
- Each Electric Vehicle shall display an orange triangle 150mm side with white EV lettering in the vicinity of the competition number (Figure K-4).



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SCHEDULE L – AUTOMOBILE LOG BOOKS

1. GENERAL

- A log book records the stamped chassis number or Vehicle Identification Number (VIN) of an automobile, its classification, its competition history and its past and present ownership.
- A log book is not evidence of the eligibility of that automobile for a particular category or group nor of compliance with event regulations.
- An automobile may be entered in a competition for a classification other than that recorded in the log book. Where this occurs it shall comply with the conditions for that competition (e.g. a 5th Category automobile entered in a contemporary competition shall comply with the technical regulations for that competition).
- Only one log book shall be valid for an automobile, as recognised by the chassis number or VIN. Any continuation log book shall carry these details. Any change of ownership or other alteration to a log book shall be made only by the Motorsport Australia National office.
- An automobile log book is required for each event in accordance with the following table:

	Club	Multi-Club	State; National; and National Other	Above National Level
Race	*	*	*	*
5th Category Race¹	*	*	*	*
Speed¹			* ²	*
Rally²	*	*	*	*
Off Road	*	*	*	*
Autotest				

NOTES:

¹ For a 5th Category Historic automobile, pending the issue of a log book, a Temporary Permit to Compete is an acceptable alternative.

² Not required for a road registered automobile.

³ A State Council may permit an automobile to compete in a club/multi-club rally without a log book, once in each calendar year. A log book is not required for a Rallysprint or a Road Rally (Touring Event).

- Where a log book has been issued it shall be presented at the event even if not required by the above table.
- Motorsport Australia may waive the requirement for a log book for a particular competition.
- If a competitor is unable to present a log book at an event where it is required the Stewards may allow the automobile to compete subject to:
 - the submission of a written declaration by the competitor that the automobile is subject of a log book;
 - the payment of a fine; and
 - the presentation of the log book to Motorsport Australia within seven days of the end of that event. Failure to do so shall result in exclusion and may incur further penalties.
- An endorsement in a log book shall be made only by:
 - the Chief Scrutineer or their deputy, the Stewards, a Technical Delegate or Motorsport Australia; or
 - in the case of a 5th Category log book, a Historic Eligibility Officer.
 Such endorsements may be cancelled or noted as having been complied with, only by one of the persons above.
- A log book may be used for the following additional purposes:
 - the recording of the placement and details of a component seal by a scrutineer, Technical Delegate or an eligibility officer;
 - the recording of a fuel tank standard and/or inspection;
 - the recording of a plumbed-in fire extinguisher standard and/or inspection;
 - the recording of an event whereby it is a requirement for the automobile to have this record for use with restricted registration in some States.
- A log book labelled or named as being issued by CAMS is recognised as being issued by Motorsport Australia.

2. FOREIGN DOMICILED AUTOMOBILES

Other than for the 5th Category, the following requirements shall apply to an automobile not domiciled in Australia.

2.1 International Events

Refer ISC.

2.2 National and Club Events

A foreign domiciled automobile which is subject of a log book issued by a foreign ASN may be allowed to compete in accordance with the following:

- The automobile may be used in Australia for a period of 12 months from the first entry in the log book at an Australian competition. If the automobile is subsequently used in an event in another country and this is noted in the log book, then the 12 month period shall commence afresh from the next Australian competition. After such 12 month period, a Motorsport Australia log book shall be required.
- A foreign domiciled automobile which is not subject of a log book and which is entered by a foreign licence holder may be allowed to compete in Australia once only without a log book after which a Motorsport Australia log book shall be required.
- For Historic Automobiles see 5th Category regulations.

2.3 Automobile with a previous competition history:

Where a vehicle is already subject of a log book Motorsport Australia may recognise the date of issue of that log book for the purpose of safety cage compliance. In such a case, it is recommended that the Motorsport Australia Technical Department be consulted.

SCHEDULE M – SCATTER SHIELDS

1. APPLICATION

A scatter shield complying with the specification below must be fitted to:

- (a) Front-engined Sports Sedans or Improved Production vehicles which have undergone an engine change, or which are fitted with a transverse engine/transmission assembly.
- (b) Front-engined Sports Cars and front-engined Category 1 automobiles, in which a derangement of the clutch-flywheel assembly could pose a hazard to the driver.

Excepted from the above requirements (a) and (b) are those cars upon which the entire clutch-flywheel housing is, in plan view, forward of all parts of the driver's body when they are seated normally in the car.

- (c) Any rear- or mid-engine Category 1 or 2 automobile:
 - (i) in which derangement of the clutch-flywheel assembly could present a hazard to the driver; or
 - (ii) which is fitted with a transverse engine/transmission assembly; or
 - (iii) on which the clutch-flywheel assembly extends to within 250mm of the rear of the driver's seat, when such seat is at the rearmost extremity of adjustment.

NOTE: Historic cars when competing in events exclusively for such cars are exempted from this requirement.

2. SPECIFICATION

The shield must be so fitted as to minimise the risk of injury in case of flywheel and/or clutch failure. It must be of safe construction.

The shield must be fitted under the floor or, in appropriate cases, in the engine compartment. It must not rely on floor or toe board for strength and it must be securely mounted. The shield must be of sufficient width and length to protect the occupant of the driver's seat.

The shield must be of a thickness of:

- (a) 6mm if mild steel plate;
- (b) 3mm if tungsten impregnated alloy steel plate;
- (c) 6mm if Alcoa aluminium alloy A5083H321. Specifications of alternative materials may be submitted to Motorsport Australia for evaluation before installation or manufacture.

3. ALTERNATIVES

A scatter shield may be dispensed with on vehicles where the construction of the flywheel/ clutch housing is such that it meets or exceeds the requirements herein, e.g. a housing fabricated from mild steel plate of 6mm thickness or one that is in compliance with the applicable SFI standard in all aspects.

A transmission blanket to the SFI Spec. 4.1 is acceptable as an alternative to one fabricated from metal as above

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To ensure you are equipped with the latest Motorsport Australia regulations, visit motorsport.org.au/regulations/manual.

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SCHEDULE N – FUEL TANKS AND REFUELING

Part 1 – Fuel Tanks (State Level and above Race Events only)

1. 1ST CATEGORY AUTOMOBILES

- (a) Each 1st Category automobile with a fuel capacity in excess of 50 litres, shall be equipped with a fuel tank/s either fitted with fuel tank foam, or fitted with a FIA safety fuel cell.
- (b) No tank filler and/or cap shall protrude beyond the coachwork. Each fuel filler must seal and prevent fuel leakage. Each air vent must be located at least 250mm to the rear of the cockpit.
- (c) Each 1st Category automobile subject to a log book issued after 1 January 1977, and any automobile in which the fuel tank is within 100mm of the outside surface of the automobile, shall incorporate a crushable structure to protect it from impact.

NOTE: the following specifications for the fuel tank crushable structure are recommended;

Minimum Thickness	1.5mm
Material	Aluminium Alloy
Minimum Tensile strength	215 MPa
Minimum elongation	5%

- (d) Each 1st Category automobile which is the subject of a log book issued after 31 December 2017 shall be fitted with an FIA safety fuel cell FT3-1999, FT3.5-1999 et FT5-1999 or shall incorporate a free design fuel tank using regulations as listed in Article 4.

2. 2ND AND 3RD CATEGORY AUTOMOBILES

- (a) Each 2nd and 3rd Category automobile, unless otherwise specified in the Group Regulations, may replace an original fuel tank/s with a FIA safety fuel cell or a free design fuel tank provided the fuel capacity does not exceed that specified below. Each automobile competing in a race scheduled to extend more than 30 minutes, which is not fitted with a FIA safety fuel cell, must have its fuel tank fitted with fuel tank foam.
- (b) From 1 January 1974, any automobile not previously registered with Motorsport Australia or of a model for which a log book has not previously been issued, must be fitted with fuel tanks of capacity no greater than specified below.
- (c) Should dry-break fuel couplings be fitted then a FIA safety fuel cell is required save for a Series Production automobile (below for exception). The fuel filling port must then be relocated so that:
 - (i) the original fuel port/s shall be rendered inoperative;
 - (ii) the valve receiver mounting must be installed in the appropriate external panel in such a way as to prevent entry of fuel into the boot compartment in the event of spillage; and
 - (iii) a flexible connection between the valve receiver mounting and the top of the fuel tank is permitted.
- (d) Each 2nd and 3rd Category automobile which is the subject of a log book issued after 31 December 2017 shall be fitted with either:
 - (i) the original fuel tank for the original automobile in the original location using at least the original fuel tank mounts; or
 - (ii) a FIA safety fuel cell FT3-1999, FT3.5-1999 et FT5-1999; or
 - (iii) a fuel tank of free design using regulations as listed in Article 4.

NOTE: See Part 2 for further information regarding refuelling with dry-break couplings.

- (e) Where dry-break fuel couplings are fitted to a Series Production automobile in which the standard fuel tank/s are retained and are totally unmodified (including baffling, fitting of foam filling and any modification to the filler neck or venting system), any consequential increase in fuel capacity shall be accepted provided that:
 - (i) the dry-break fuel coupling/s and venting components are fitted as close as practical to the fuel tank;
 - (ii) the ID of the connecting tube between the dry-break coupling and the original filler neck of the fuel tank is not greater than the OD of the exit of the dry-break coupling/s; and
 - (iii) the filler and vent tubes between the fuel tank/s and dry-break coupling/s are as short and as direct as practical.
- (f) From the 31 December 2017 each 2nd and 3rd Category automobile which is fitted with dry-break fuel couplings shall have fitted:
 - (i) a vent system with a gravity activated roll-over valve;
 - (ii) a fill plate with one way valves to prevent fuel leakage;
- (g) Any installation of LP Gas to an automobile must be in compliance with the requirements of AS 1425 (1989). Each fuel tank/s may be made demountable utilising appropriate self-sealing connections on fuel hoses.
- (h) The maximum capacity of fuel tank fitted to an automobile of 2nd, and 3rd Categories, except as provided above, shall be:

Up to 700cc	60 litres
701-1000cc	70 litres
1001-1400cc	80 litres
1401-1600cc	90 litres
1601-2000cc	100 litres
2001-2500cc	110 litres
Over 2500cc	120 litres

3. GENERAL

- (a) Except where the Group/category/class regulations or event Supplementary Regulations require compliance with Schedule N, the conditions of Schedule N, Part 1 are recommended but not mandatory for an automobile in a Club or Multi-Club race.
- (b) An automobile which is subject to a log book issued after 1 January 1977 are not permitted to have fuel tanks forming part of the stressed structure of the automobile.
- (c) Refuelling in pit lane during a race must be conducted in compliance with Part 2 of this Schedule.
- (d) Refer also to relevant technical regulations for the Group/category/class which may apply additional requirements.

4. FREE FUEL TANK DESIGN

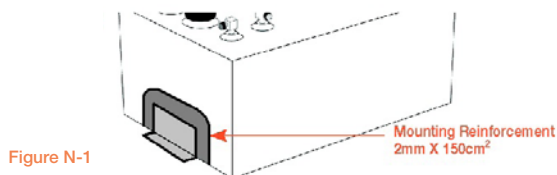
- (a) A free design fuel tank shall:
 - (i) be constructed of steel, stainless steel, or 5005 aluminium, a minimum 2mm thickness. Specific category regulations may vary the material type and dimensions.

NOTE: A fuel tank constructed of a non-metallic material is not permitted as a fuel tank of free design.

An automobile that is fitted with a non-metallic tank which may otherwise comply with the requirements for a fuel tank of free design is permitted to be used until the 31 December 2020.

The use of a non-metallic fuel tank of free design will not be permitted as of 1 January 2021.

- (ii) be mounted by a cradle and strap/s or, if fixed by mounting tabs, each tab shall have a mounting reinforcement of a minimum 2mm x 150cm² with large radius corners welded to the tank; (Fig 1)
- (iii) be mounted by a cradle and strap/s or, if fixed by mounting tabs, each tab shall have a mounting reinforcement of a minimum 2mm x 150cm² with large radius corners welded to the tank; (Fig N-1)



- (iv) contain fuel-resistant polyurethane foam baffling, conforming to mil spec mil-b-83054, sae-air-4170 or equivalent.
- (v) Where rapid refuelling is expected, anti-static foam conforming to mil-spec MIL-F-87260 (USAF) shall be fitted;
- (vi) incorporate a vent system with a gravity activated roll-over valve;
- (vii) (if the tank is remotely filled) incorporate a fill plate with one way valves to prevent fuel leakage; and
- (viii) be internally inspected for safety foam deterioration every 2 years.

5. FUEL CELL INSPECTION

- (a) Each automobile in an international competition shall comply with the FIA ISC.
- (b) The following regulations of this article (5(b)) shall apply from 1 January 2018 and only to an automobile competing in an event permitted by Motorsport Australia. A FIA safety fuel cell FT3, FT3.5 and FT5 shall be inspected in compliance with the following requirements:
 - (i) Inspection of a fuel cell shall become due on the FIA expiry date of 5 years after manufacture;
 - (ii) Inspection of a fuel cell shall be carried out by a Motorsport Australia approved test facility every 2 years, refer Article 6;
 - (iii) Maximum life of a fuel cell shall be 15 years from the date of manufacture;
 - (iv) A damaged fuel cell shall not be repaired;
 - (v) Proof of inspection must be supplied to a scrutineer on request; and
 - (vi) Test details are to be recorded by Motorsport Australia in the Log Book change of details section.

NOTE: Group/category/class or event regulations may apply a higher standard for a fuel cell.

6. APPROVED FUEL CELL TEST FACILITIES

Australian Fuel Cells 14/4 Transport place Molendinar QLD 4214 (07) 5597 1533	Racer Industries 75 Norwell road Norwell QLD 4208 1300 738 553	Bond Roll Bars 3/6 Precision place Park road industrial estate McGrath's Hill NSW 2756 (02) 4587 9672
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NOTE: Test facilities may be added to this list on application to Motorsport Australia.

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SCHEDULE N – FUEL TANKS AND REFUELING

Part 2 – Refuelling in Pit Lane

1. GENERAL

- (a) For any refuelling undertaken in pit lane, during any testing, practice, qualifying or race, the following procedures must be adopted, unless Supplementary Regulations provide otherwise.
- (b) Any refuelling in a pit or paddock area not utilising these procedures must be undertaken either with an automobile totally within a pit garage or in the area of the paddock specifically designated for the purpose of refuelling; and must be completed with the engine turned off.
- (c) Under no circumstances will refuelling be permitted on the circuit.
- (d) When event regulations allow the use of any alternative refuelling equipment, those regulations shall prescribe all relevant conditions, including capacity of permitted refuelling containers and the requirements for delivery hose/s.
- (e) Refuelling shall only be undertaken by an overhead rig and or an alternate method approved by Motorsport Australia and detailed in the Group/category/class regulations or event Supplementary Regulations.
- (f) Refuelling in pit lane is not permitted unless it is specifically authorised in the Supplementary or Further Regulations for the event.
- (g) For international events, refer to the supplementary regulations and the FIA ISC.

2. SAFETY

- (a) **AREA:** The area of the pit garage in which the fuel reservoirs are situated must be adequately ventilated and have unimpeded access from front and rear of the pit garage. A minimum quantity of extinguishment (9kg if dry powder type), which must be in either one or two extinguishers must be available for use in each pit. The area must be clean and free of potentially flammable materials e.g. paper, rags, oily fabrics etc.
- (b) **PIT CREW:** As prescribed in the Circuit Race Standing Regulations, the maximum number of attendants handling refuelling-related procedures will be four, their designated tasks will be as follows:
 - (i) Fire attendant;
 - (ii) Two hose attendants;
 - (iii) One emergency cut off valve attendant.All attendants will be required to be attired as per Schedule D, Article 3.6. Balacavars must be worn and any resultant exposed areas (e.g. eyes) must be covered to minimise risk of injury from flash fire burns (e.g. by goggles). The wearing of a full face helmet is recommended in addition to the above.
Any other pit crew members whose tasks place them within one metre of the refuelling valve on an automobile will be required to wear clothing as listed above for attendants.
For the entire time of a pit stop during which any refuelling operation is undertaken, it will be compulsory for the fire attendant to be ready to attend to any fire which may occur. The fire attendant must stand near the automobile and the overhead rig, poised with the extinguisher ready to operate.
- (c) **EMERGENCY CUT-OFF VALVE:** It will be compulsory for any refuelling operation, which incorporates an overhead rig, that the rig be equipped with a ball-cock or similar fast action valve which must work on the "dead man" principle. This valve must be attached directly to the fuel reservoir. See Diagram N-2. The emergency cut-off valve attendant must hold the valve open only during the refuelling operations. When pressure on the handle of the cut-off valve is released, the valve must immediately close, stopping the flow of fuel from the reservoir. The closing principal must not rely on the action of gravity alone.
- (d) **APPROVAL OF INSTALLATIONS AND EQUIPMENT:** All installations and equipment must be specifically approved by the Chief Scrutineer or their nominee prior to any competition during which refuelling is permitted in pit lane. Event Supplementary Regulations will provide further details of times etc. for any inspection.
- (e) **EARTHING:** During any refuelling operation, it is strongly recommended that the automobile be electrically connected to earth, as follows:
 - (i) Each pit is equipped with two grounding connections. The automobile must be equipped with a suitable terminal to which one of these connections may be attached.
 - (ii) The refuelling system (including tower, tank, hose, nozzle, valves and vent) must be connected to one of the grounding connections detailed above for the duration of the race.
 - (iii) The automobile must be connected to the other grounding connection for the duration of all refuelling operations.
 - (iv) No fuel hose connection may be made unless conditions (ii) and (iii) have been fulfilled.
- (f) **AUTOMOBILE MOVEMENT:** The automobile must be stationary at all times during refuelling.

3. FUEL TANKS (AUTOMOBILE)

Refer to Schedule N (Part 1) and the relevant Group/category/class regulations for requirements of fuel tanks.

4. STANDARD COUPLING

Unless specifically authorised otherwise by Motorsport Australia and published in the event regulations, all refuelling operations must be carried out utilising a Siamese dry-break system. A dry-break system is deemed to consist of two separate units, the receiving unit and the probe unit together with the associated hoses, valves etc. The general design of the receiving unit and probe unit is included at Diagram N-1. A male probe unit shall be fitted to each refuelling hose. The receiving units shall be fitted to the automobile, under the conditions listed in the technical regulations for the relevant Group/category/class.

It is permitted to only use one receiving valve to permit the entry of fuel into the tank, and one receiving valve for connection to the return vent.

Maintenance: Competitors are advised that it is extremely important to maintain all refuelling equipment in good working order. O-rings must be regularly inspected and replaced if there are any signs of expansion or damage. Springs and tracks must also be regularly inspected and kept lubricated during those times the refuelling valves are not in operation.

5. REFUELLING

Refuelling shall only be undertaken by an overhead rig.

- General:** This system envisages fuel being stored in a single rigid tank of fixed capacity (see Diagram N-2), at a specific maximum height above the pit lane. An automobile will be refuelled via a single flexible hose which in turn is fitted with a male probe unit of standard design (see Article 3) with a single return vapour line to the overhead rig.
- Design Requirements:** The maximum height of any part of the rig which contains fuel is two metres above the pit lane. Only non-fuel holding connections and vents are permitted above two metres.
- The maximum capacity of the reservoir including the delivery hose is 220 litres which must in all circumstances allow 10% ullage (air space).
- A single delivery hose, which must be of a flexible rubber or a fuel resistant reinforced plastic material must be connected to the "dead man" valve.
- The flexible part of the hose must be at least 2.5m in length and of an ID no greater than 50mm (2").
- The flexible hose must be connected to the male probe. A solid connecting sleeve (maximum length 300mm) is permitted between the hose and the probe, to assist with ease of handling.
- All parts of the refuelling system, including the male probe, the flexible hose, any connection sleeve, the cut-off valve and the reservoir must be electrically continuous and efficiently grounded.
- The reservoir must be vented via an explosion safe shielded vent.
- A filling orifice (maximum dimension 50mm) may be fitted to the main reservoir. Any refilling operations to the main reservoir must be carried out bearing in mind civil Occupational Health and Safety regulations.
- No artificial pressurisation of the reservoir is permitted.
- The vent must be open to the atmosphere and only atmospheric pressure may be exerted on the fuel in the main reservoir.
- Any device which changes the ambient temperature of the fuel is prohibited.
- The rig shall not be moved once filled and must be completely drained before moving.
- The fire attendant must be "ready to attend" during any refilling operations.
- Use:** The reservoir, return and delivery hose, must remain in pit lane, close to the garage roller door at all times when in use.
- The reservoir must be securely tethered from at least 2 opposing points on top of the reservoir to a structurally suitable point, located above the reservoir, which is substantial enough to support the reservoir when full.

6. VENTING DURING REFUELLING OPERATIONS

In all refuelling operations, the fuel tank/s of the automobile must be vented through a standard coupling (see Article 4), by a return vent integrated with the filler coupling as a Siamese unit.

Return Vent:

- General:** The return vent allows a fuel tank of an automobile to be vented to the ullage space in the reservoir of the overhead refuelling rig (see Diagram N-2).
- Design Requirements:** Maximum internal diameter at vent hose is 50mm (2").
- The vent hose must only return to the reservoir tank.
- The reservoir of the main tank must be vented externally to any pit garage.

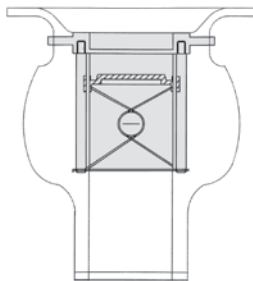
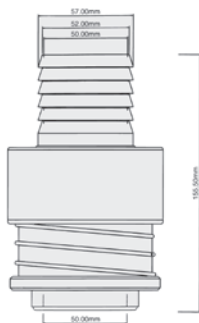


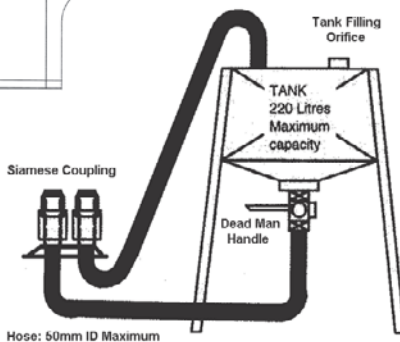
Diagram N-1:

General design of dry-break coupling

Standard coupling (probe and receiver unit)

Diagram N-2:

Overhead rig with fuel hose and return vent





2019 St George 399
Photo: Terry Hill Photography



2019 Shannons Motorsport Australia Championships - Sandown Raceway
Photo: Speed Shots Photography



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